

Huntmar Drive Widening (Campeau Drive to Maple Grove Road) and Stittsville Main Street Extension (Maple Grove Road to Robert Grant Avenue) Environmental Assessment (EA) Study

Introduction

Welcome to the second and final Public Consultation Event for the Huntmar Drive Widening and Stittsville Main Street Extension Environmental Assessment (EA) Study.

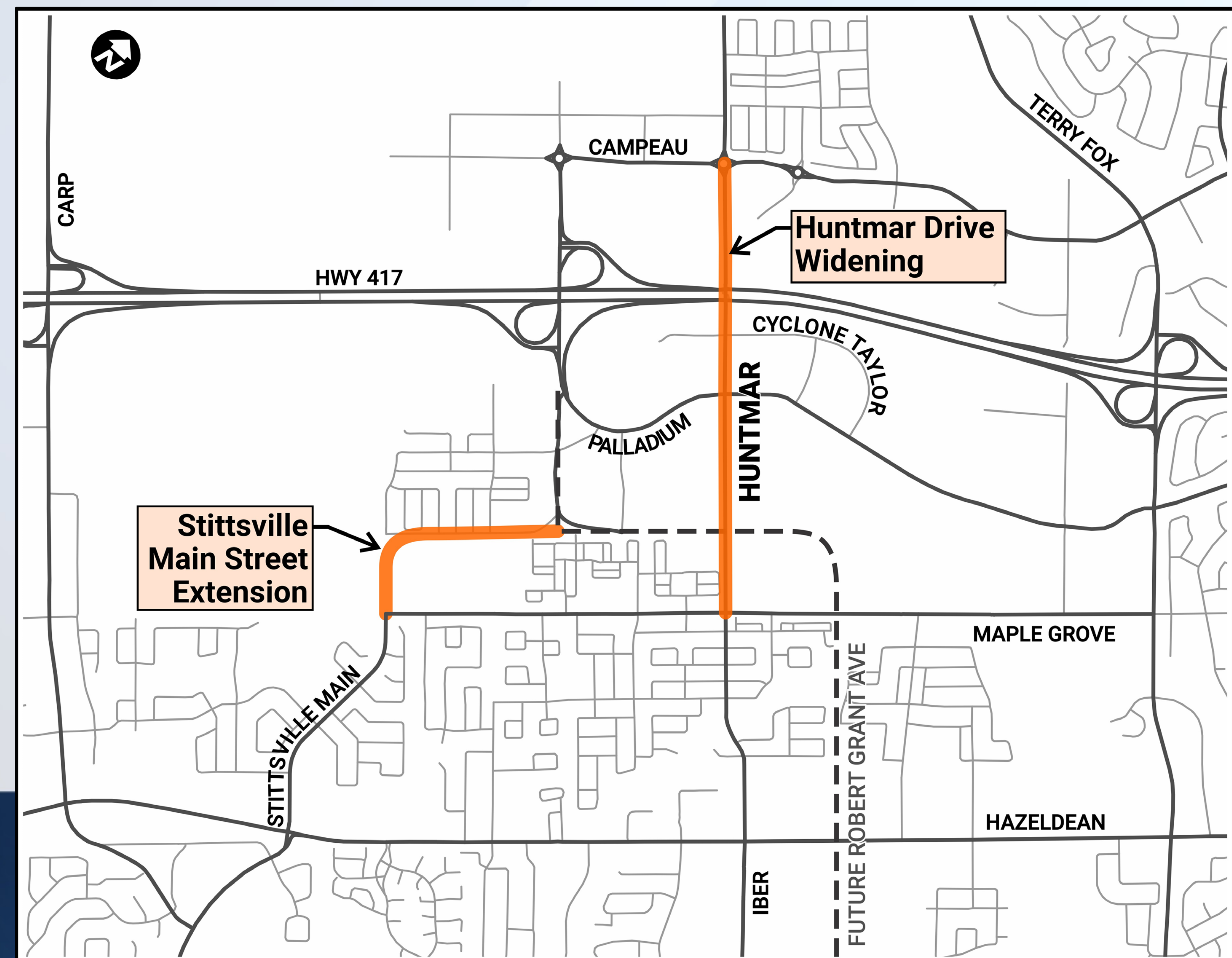
The project limits for the widening of Huntmar Drive start from Campeau Drive in the north and extend to Maple Grove Road in the south (approximately 1.7 km). The limits of the Stittsville Main Street Extension are from Maple Grove Road to Robert Grant Avenue. The projects limits are shown in the adjacent figure.

The City's Transportation Master Plan (2013 TMP) identifies the widening of Huntmar Drive and the extension of Stittsville Main Street as part of the future roadway network in the area.

The City is welcoming feedback on the information presented at this time on the Recommended Plan and Functional Design **from January 31 until February 23, 2023.**

Details of how to provide feedback are provided on the city's project website: [Ottawa.ca/huntmardrivestittvillemain](https://ottawa.ca/huntmardrivestittvillemain)

Your input is important to the success of the study!



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Land Acknowledgement

- Ottawa is located on unceded territory of the Algonquin Anishinabe Host Nation.
- The peoples of the Algonquin Anishinabe Host Nation have lived on this territory for millennia.
- Today, Ottawa is home to approximately 40,000 First Nations, Inuit and Métis people.
- Ottawa's Indigenous Community is diverse, representing many nations, languages and customs.
- The City honours the land of the First Peoples, as well as all First Nations, Inuit and Métis in Ottawa and their valuable past and present contributions to this land.

Reconnaissance du territoire

- Ottawa est située sur un territoire non cédé de la nation hôte algonquine Anishinabe.
- Les peuples de la nation algonquine Anishinabe vivent sur ce territoire depuis des millénaires.
- Aujourd'hui, Ottawa compte environ 40 000 membres des Premières Nations, Inuits et Métis.
- La communauté autochtone d'Ottawa est diverse et représente de nombreuses nations, langues et coutumes.
- La Ville rend hommage au territoire des premiers peuples, ainsi qu'à l'ensemble des membres des Premières Nations, des Inuits et des Métis d'Ottawa, de même qu'à leurs précieuses contributions passées et présentes à ce territoire.

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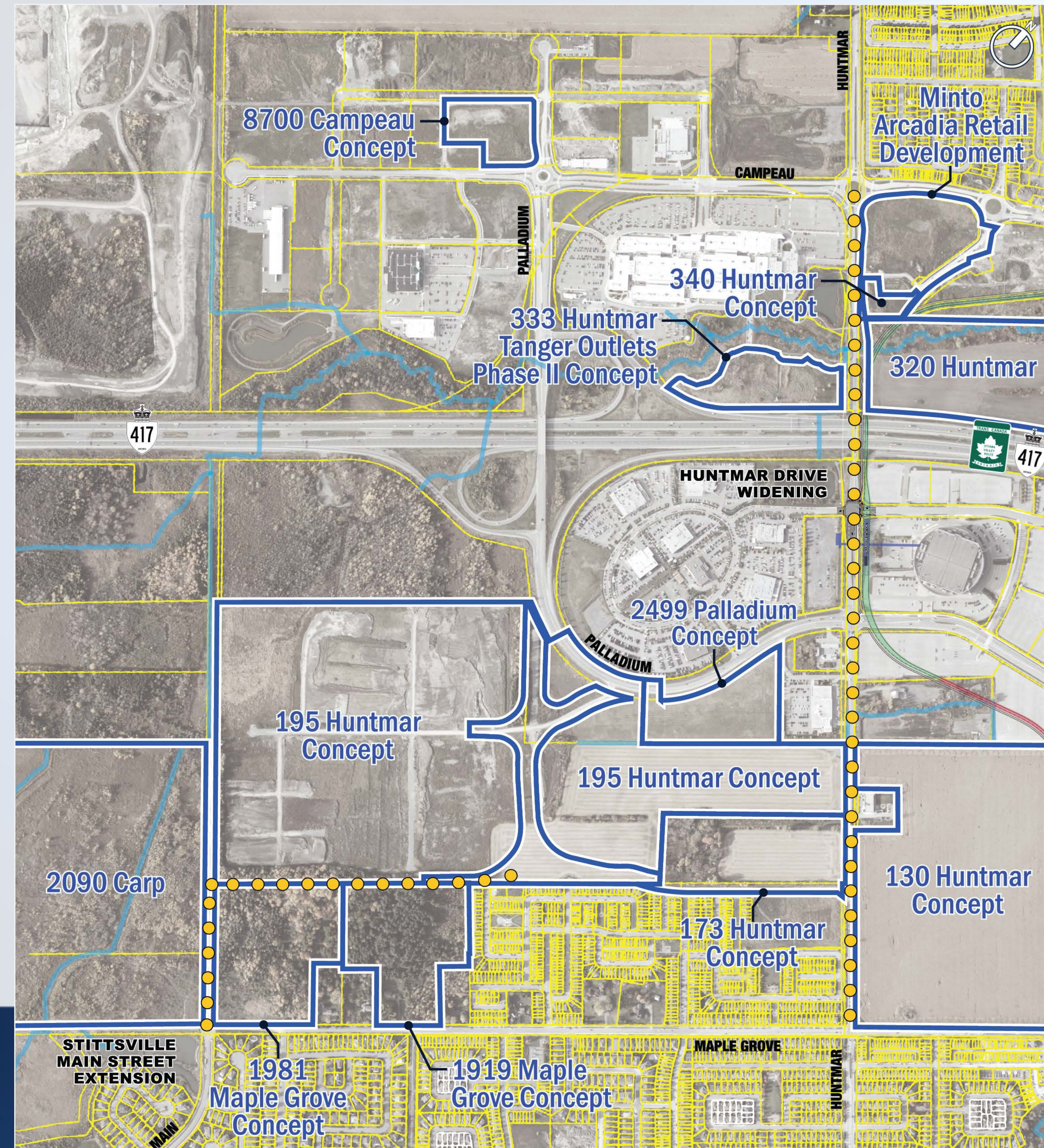
Study Objectives

- **Investigate** opportunities to improve Huntmar Drive for all modes (including bridge crossing opportunities over Highway 417), and for the extension of Stittsville Main Street, both as complete streets
- **Accommodate** all travel modes by including accessible features for walking, cycling, transit, autos and trucks that are compatible with surrounding land uses and developing communities
- **Consult** with a broad range of stakeholders, collect and consider feedback
- **Coordinate** with area developments and preliminary designs including the LRT extension and the planned extension of Robert Grant Avenue
- **Establish** right-of-way requirements and project cost estimates
- **Prepare** a Recommended Plan and Environmental Study Report

Study Area

Huntmar Drive – A range of land uses exist along the corridor, including the Canadian Tire Centre, retail, commercial, employment uses, housing, a private school, and vacant lands which are being planned for development.

Stittsville Main Street – This corridor is surrounded primarily by vacant lands which are being planned for residential development.



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Environmental Assessment Process

The Recommended Plan has been developed and evaluated in accordance with the requirements of the Ontario Municipal Class EA process for a Schedule C project.

Upon completion, this Study will address Phases 1 to 4 of the Class EA Process:

Phase 1: Confirm the project need or opportunity.

Phase 2: Evaluate alternative solutions.

Phase 3: Evaluate alternative designs.

Phase 4: Complete an Environmental Study Report (ESR) that documents the study process and includes recommended mitigation measures and future commitments.

Phase 1 and 2 were originally completed as part of the 2013 TMP. As part of this Study, these two phases were reviewed and re-confirmed. Following completion of Phases 1 through 4, the Recommended Plan will be presented to the City's Transportation Committee and Council for approval.

The project will then be considered to have EA approval. It will be able to proceed to Phase 5 (implementation), once funding is in place.

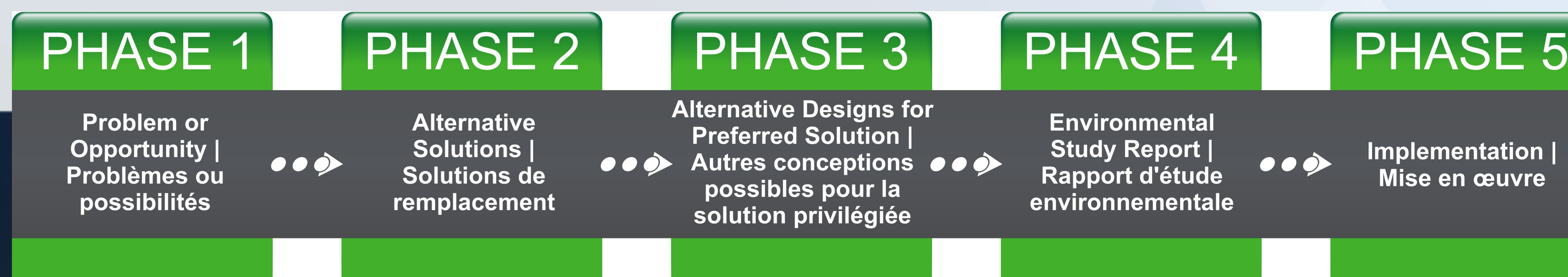
Consultation

Stakeholder input occurs throughout the study process and includes:

- Three sets of consultation group meetings with agencies, community associations, special interest groups, landowners, businesses, and Accessibility Advisory Committee
- Two public consultation events
- Meetings with individual stakeholders on an as-needed basis
- Website for the project: Ottawa.ca/huntmardrivestittvillemain
- Consultation with Indigenous Communities
- Master mailing list to receive updates on the project by request
- 30-day public review period of the completed ESR

Schedule

Consultation Group Meetings #1	Fall 2021
Consultation Group Meetings #2	Winter/Spring 2022
Public Open House #1	June 2022
Consultation Group Meetings #3	Fall 2022/Winter 2023
Recommended Plan to Transportation Committee	Spring 2023
30-day Public Review Period of Environmental Study Report (ESR)	Spring 2023



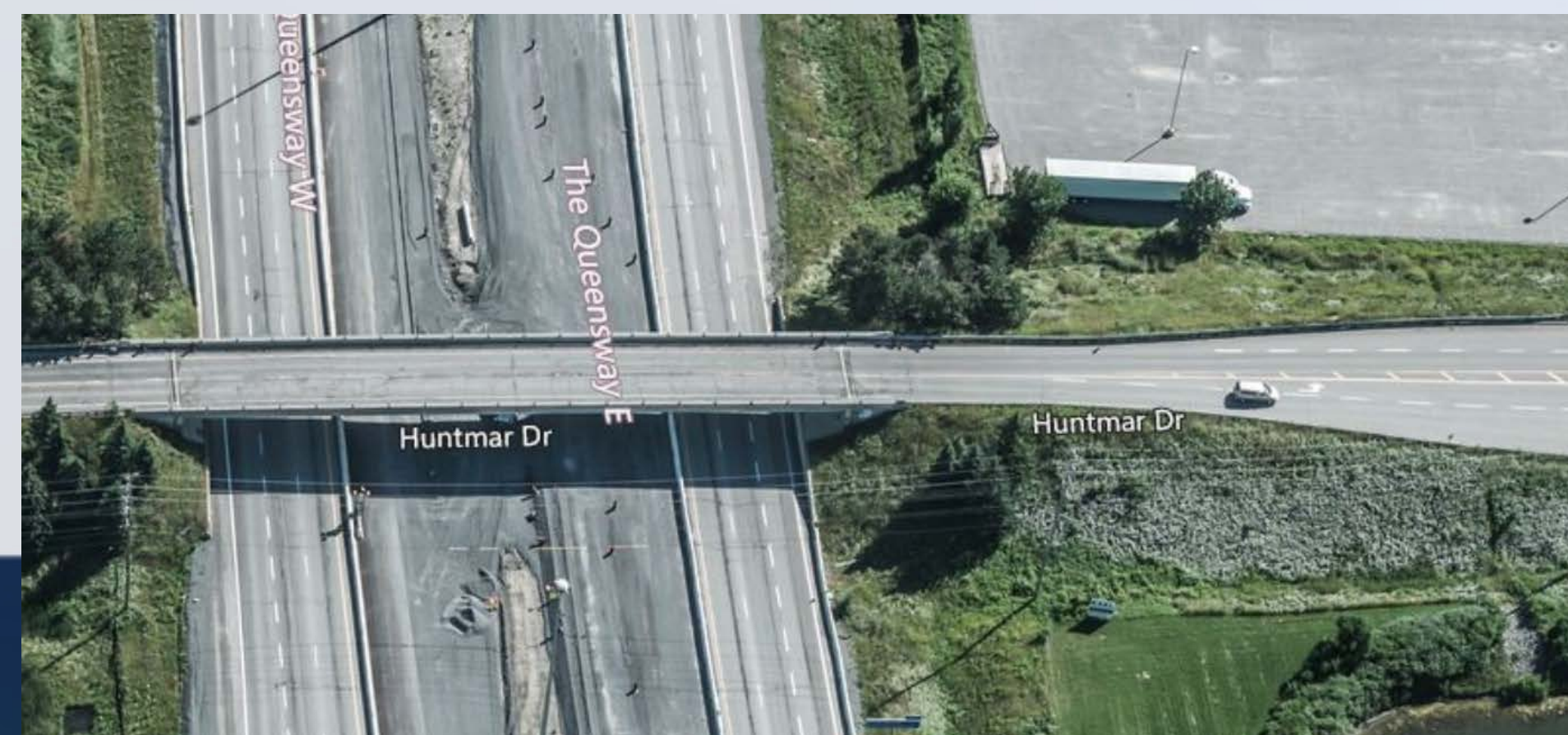
We are here!
(January 2023)

Huntmar Drive Widening (Campeau Drive to Maple Grove Road) and Stittsville Main Street Extension (Maple Grove Road to Robert Grant Avenue) Environmental Assessment (EA) Study

What We've Heard so Far

Discussions and feedback received to-date has focused around the following themes:

- Need to provide transportation capacity to serve the area; coordinate with planned and active subdivision developments
- Strong support to construct both corridors as complete streets featuring accessible sidewalks, cycle tracks and transit amenities
- Strong support for a safe active transportation crossing of Highway 417



- Support for the safety and accessibility for all users of all ages and abilities to be considered in the design
- Desire for enhanced landscaping and tree planting throughout both corridors
- Support for emphasizing sustainability in design, considering user experience related to future climate change influences
- Connections for all modes to/from developing areas.
- Questions regarding how soon the project can be built and phasing opportunities

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Need and Opportunity

Huntmar Drive:

There is a **need** to:

- Provide additional travel capacity to accommodate growth and development in surrounding communities.
- Improve crossing of Highway 417 including an opportunity for early implementation of an active transportation bridge.
- Maintain the Arterial Road function, joining Campeau Drive to Maple Grove Road, with a crossing of Highway 417.

Stittsville Main Street:

There is a **need** to:

- Implement a Major Collector Road to serve new development, connect communities to the existing and future transportation network.

There is an **opportunity** for both roads to be designed as healthy complete streets, showcasing accessibility, active transportation and transit.

Recommended Solutions

Following an evaluation process of all reasonable alternative solutions for both corridors, and responding to the **need and opportunities** presented, the preferred solutions were reconfirmed as presented in the 2013 TMP:

The Preferred Solution for **Huntmar Drive** is to reconstruct as a complete street.

The Preferred Solution for **Stittsville Main Street** is to extend as a complete street providing for all modes from Maple Grove Road to the future Robert Grant Avenue.

The Preferred Solution for **Huntmar Drive/Highway 417 Crossing** is to replace the existing structure with a single bridge crossing in its existing location and to provide for the opportunity of an early active transportation bridge.



Arterial - Existing	—	Artère - Établie
Arterial - Future (alignment defined)	- - -	Artère - Future (alignement déterminée)
Major Collector - Existing	—	Grande collectrice - Établie
Major Collector - Future	- - -	Grande collectrice - Future
Collector - Existing	—	Collectrice - Établie
Collector - Future	- - -	Collectrice - Future
River Crossing (corridor undefined)	- - -	Traversée de rivière (couloir non défini)

Huntmar Drive Widening (Campeau Drive to Maple Grove Road) and Stittsville Main Street Extension (Maple Grove Road to Robert Grant Avenue) Environmental Assessment (EA) Study

Complete Street Approach



The Recommended Plan for Huntmar Drive and Stittsville Main Street has incorporated the City's complete street approach.

- Council approved the Complete Streets Implementation Framework (2015) which guides the planning, design, operations and maintenance of road infrastructure
- Recognizes roads as public spaces, and the need to provide safety, comfort and convenience to users of all ages and abilities, regardless of their mode of transportation
- The City is committed to developing networks of complete streets as opportunities arise through road renewal and new construction such as this project
- All streets can be a Complete Street, but specific recommendations may vary according to context

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Urban Design and Placemaking

The City's new 2022 Official Plan identifies "Hubs" and "Corridors" that exist within the study area. These designations have guided the design of the Recommended Plan for Huntmar Drive. Specifically, the Hub designation is at the Palladium lands (Canadian Tire Centre) and means the design of this area should include:

- Focus on planned or existing rapid transit stations
- Diverse functions, mixed uses
- Higher density of development
- Higher level of transit connectivity
- Major employment centres

Corridors (Minor) designation is also located along Huntmar Drive and these areas mean:

- Bands of land along specified streets, planned for higher density of development and a mix of uses.
- More dense than surrounding neighbourhoods, but less than hubs

There are placemaking opportunities to provide for unique expressions and urban design. The toolkit in designing these spaces might include:

- Wayfinding Signage
- Distinctive Surfaces
- Benches
- Pedestrian Scale Lighting
- Public Art
- Garbage and Recycling bins

Landscaping and Shade Trees are included in the placemaking toolkit. The landscaping strategy for the Recommended Plan includes:

Landscaping Strategy

The landscaping strategy will consider what is suitable to the street context and in consultation with the City's Forestry Department. Specific to Huntmar Drive, wide planting zones in boulevard areas are included to provide for landscaping. A "Gateway" feature in the 'hub' area near the Canadian Tire Centre will utilize landscaping to designate the space. Wherever possible, the landscaping will be enhanced to provide shade and shelter for users.



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Accessibility in the Design

Key Design Considerations

The key design considerations that went into developing the Recommended Plan are:

- Safe, equitable, accessible design considering all modes and users
- Pedestrian and cycling facility connectivity
- Transportation operations and level of service, transit amenities
- Surrounding land uses and future developments
- Urban Design including “Hub” designation
- Climate Change
- Coordination with existing functional design for LRT and the Palladium Station
- Landscaping/streetscaping
- Stormwater management
- Access and turning movements
- Traffic calming and parking opportunities
- Respect for, protection of the natural environment context
- Right-of-way and property requirements

The Recommended Plan has considered interim measures to improve the transportation environment in the study area.

Public Spaces are to be inclusive and accessible to everyone of all ages and abilities.

The project has been designed to meet the *Accessibility for Ontarians with Disabilities Act* and the City of Ottawa’s *Accessibility Design Standards*. Consideration will also be made for the *Federal Accessible Canada Act*.

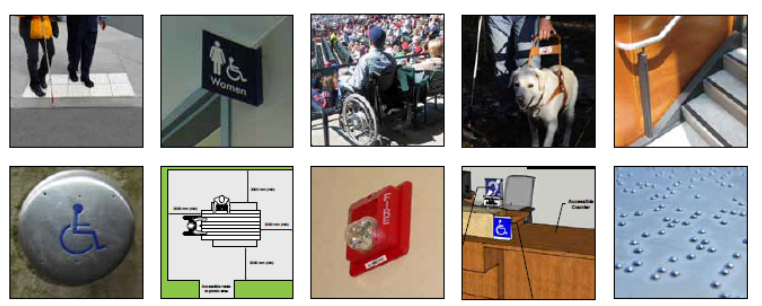
Key considerations in the design phase with respect to Accessibility include:

- Type of pedestrian and cycling facilities
- Length and type of crosswalks
- Passenger loading areas
- Tactile Walking Surface Indicators (TWSI)
- Unobstructed sidewalks
- Ground and floor surfaces
- Resting areas

The City’s Accessibility Advisory Committee is a participant on this Study.



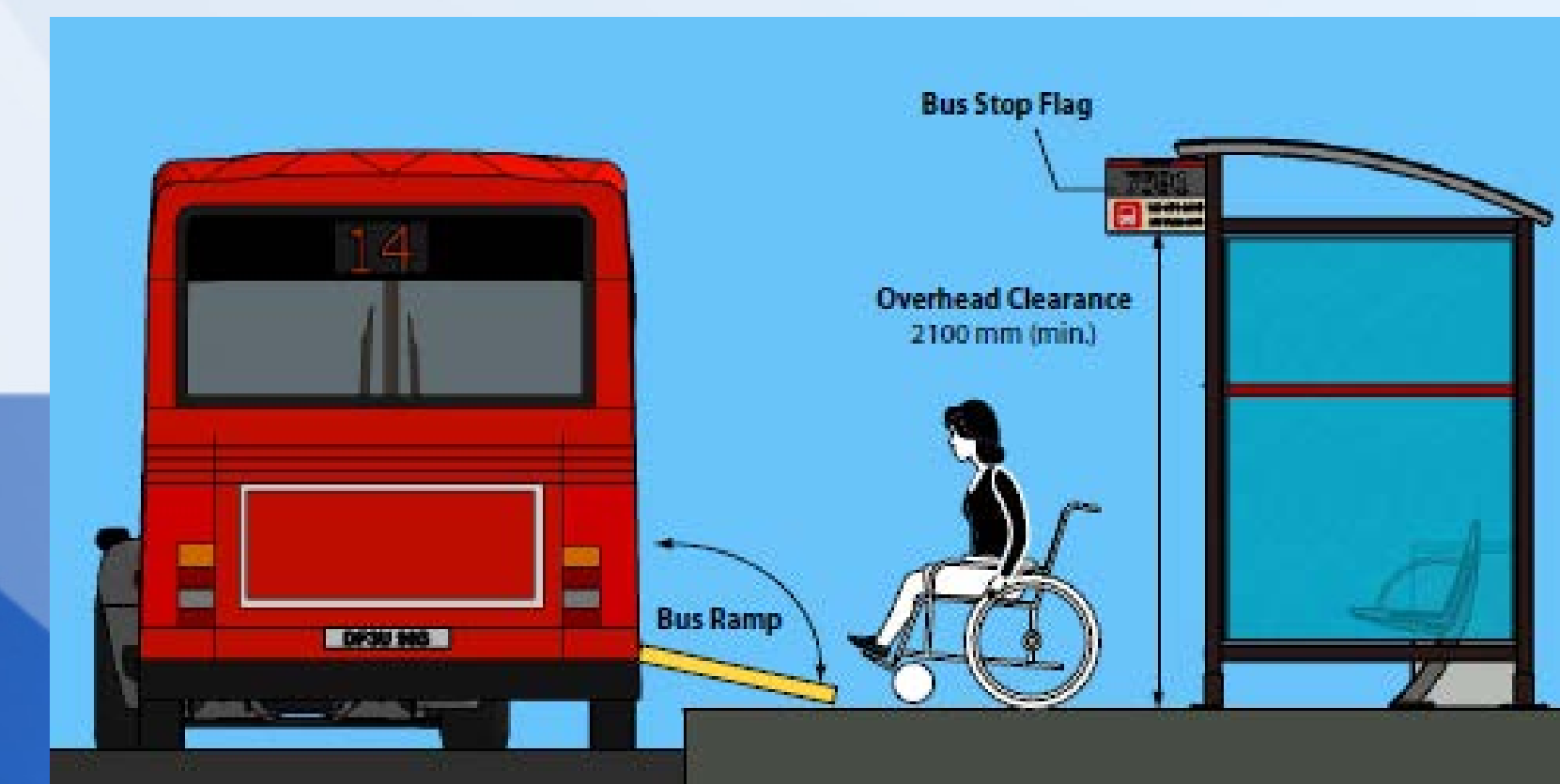
Accessibility Design Standards



Second Edition, November 2015



ACCESSIBLE CANADA ACT



Huntmar Drive Widening (Campeau Drive to Maple Grove Road) and Stittsville Main Street Extension (Maple Grove Road to Robert Grant Avenue) Environmental Assessment (EA) Study

Project Benefits

There are a number of benefits to the implementation of the project including:

- **Provides complete streets** that **recognize and meet the diverse mobility needs of all residents, businesses and visitors**.
- Separates active transportation facilities, **improving safety** and **promoting better public health**.
- **Integrates transportation and land use**, connecting existing and future developments to wider transportation network, including the future Kanata LRT extension.
- Provides sufficient transportation capacity and provides multiple modal choices; enables economic activity and **enhances the economy**.
- **Protects the environment** through context-sensitive design, attention to surrounding natural features, and consideration for opportunities to **enhance the environment** through landscape design including tree planting.

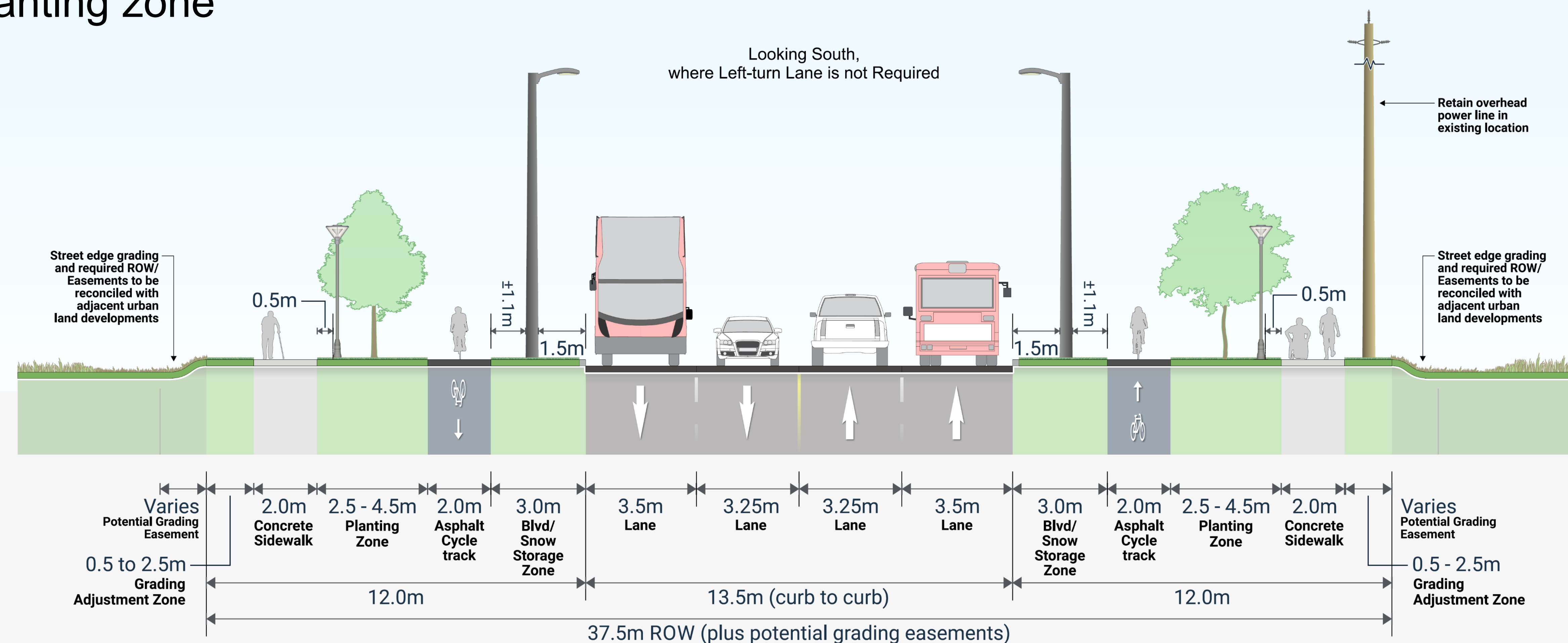


Huntmar Drive Widening (Campeau Drive to Maple Grove Road) and Stittsville Main Street Extension (Maple Grove Road to Robert Grant Avenue) Environmental Assessment (EA) Study

Huntmar - Draft Recommended Plan

The proposed cross-section features:

- **4-lane undivided roadway** (2 lanes in each direction)
- **Wide inner boulevards** for snow-storage, buffer to active transportation facilities
- **2m unidirectional cycle tracks**, **2.0-2.5 m sidewalks** on both sides
- Option to separate cycle track, sidewalk with a landscaped planting zone; or to combine them
- Pedestrian scale lighting in the planting zone
- Space for tree planting
- Bus stop platforms
- Benches



Huntmar Drive Widening (Campeau Drive to Maple Grove Road) and Stittsville Main Street Extension (Maple Grove Road to Robert Grant Avenue) Environmental Assessment (EA) Study

Huntmar Active Transportation Bridge - Draft Recommended Plan

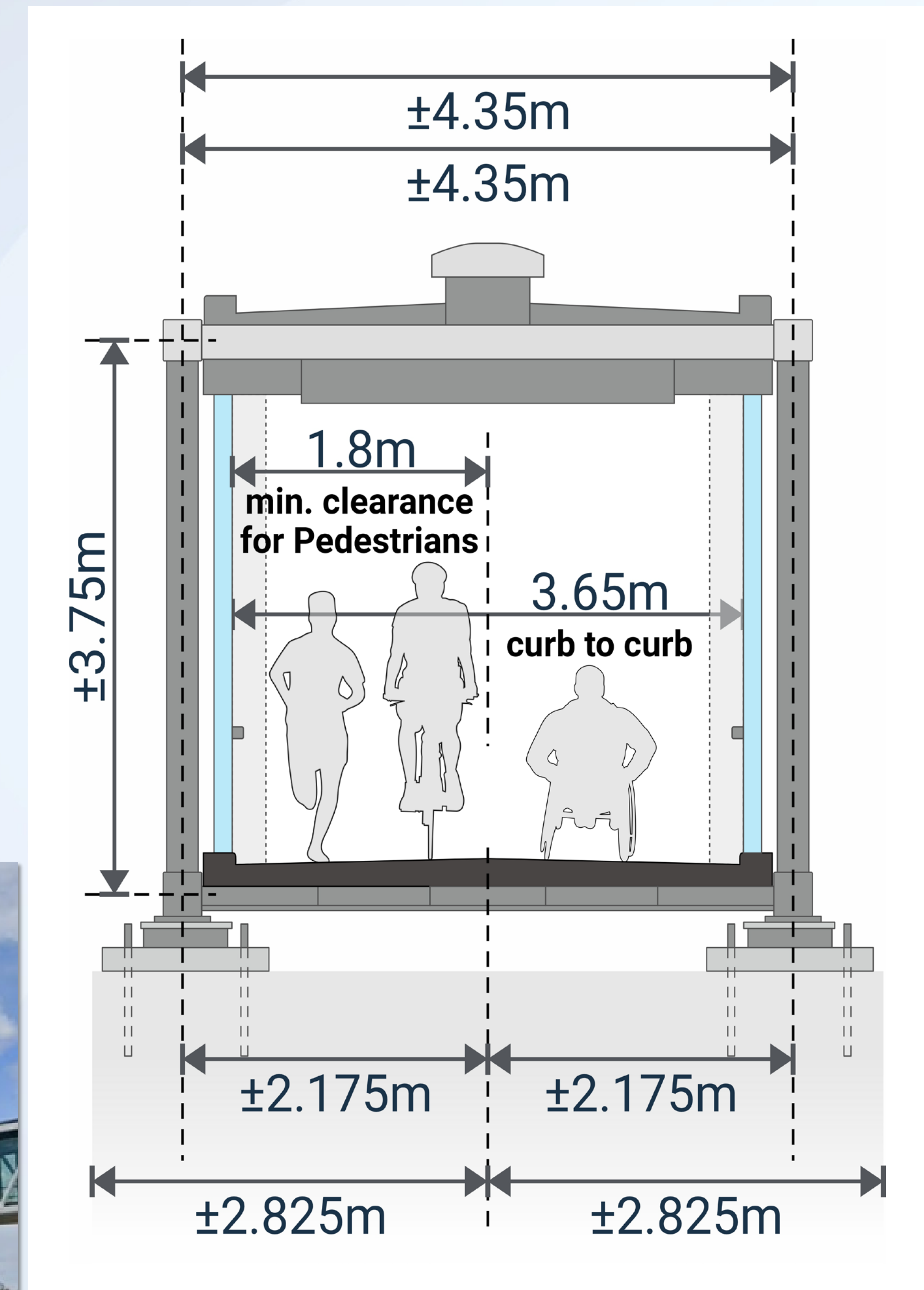
The study has identified the need and opportunity for an active transportation (AT) bridge to be constructed across HWY 417 on the following basis:

- As the timing of the new complete street Huntmar Drive bridge is not established and because it will be a notable investment, an AT bridge could provide a connection for pedestrians and cyclists earlier
- The AT bridge can be constructed in the space that will be available between the planned LRT bridge and the new proposed complete street Huntmar Drive bridge

The image at the right shows the proposed cross-section and dimensions of the AT bridge.

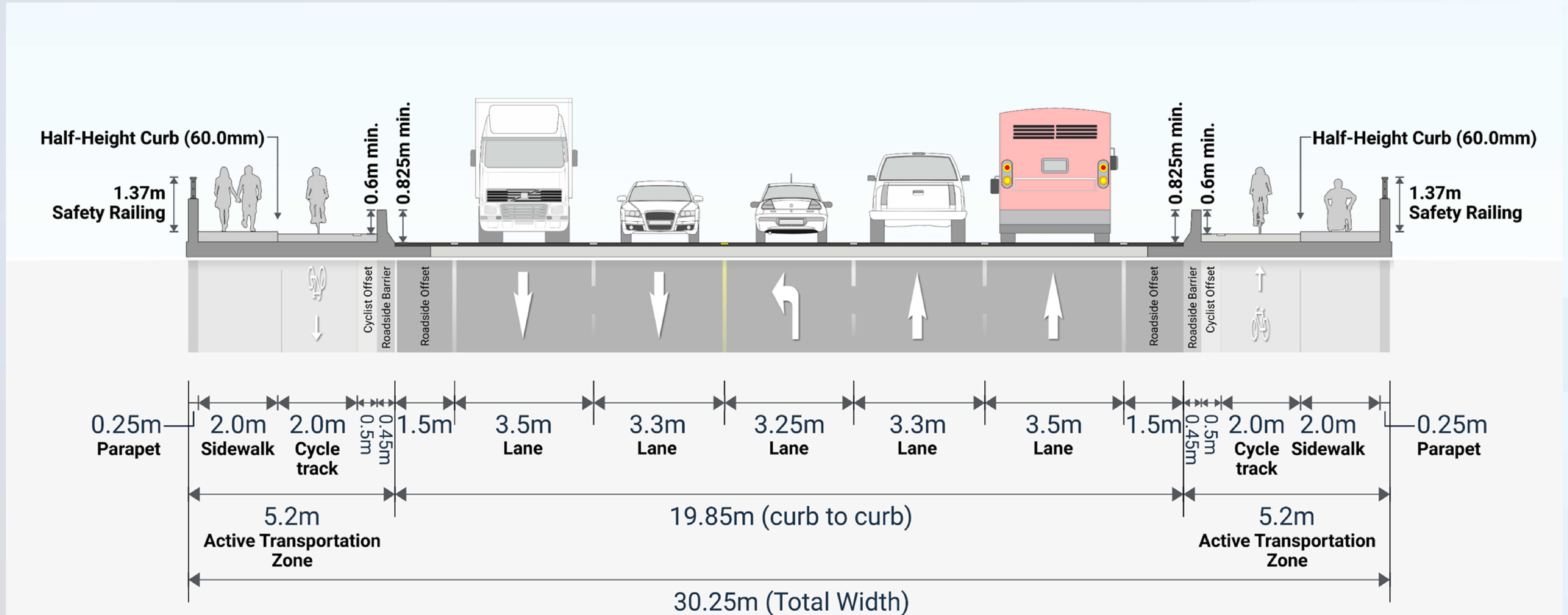


Max Keeping Bridge, Ottawa



Huntmar Drive Widening (Campeau Drive to Maple Grove Road) and Stittsville Main Street Extension (Maple Grove Road to Robert Grant Avenue) Environmental Assessment (EA) Study

Huntmar Complete Street Bridge - Draft Recommended Plan



March 2022

Huntmar Drive Widening (Campeau Drive to Maple Grove Road) and Stittsville Main Street Extension (Maple Grove Road to Robert Grant Avenue) Environmental Assessment (EA) Study

Stittsville Main Street - Draft Recommended Plan

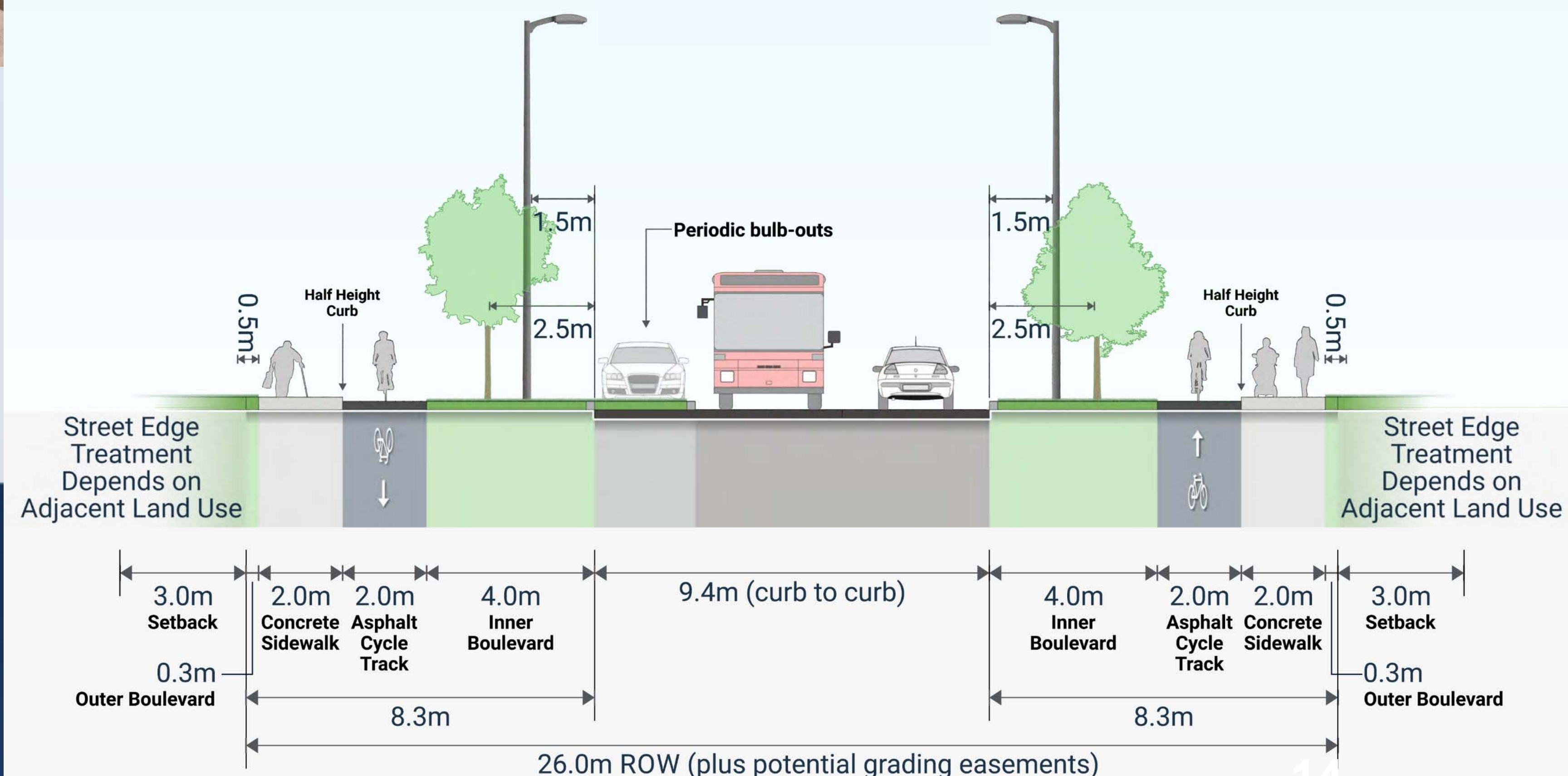


Artistic rendering of the Recommended Plan for Stittsville Main Street

The Draft Recommended Plan provides the opportunity for traffic calming, street-tree growth, and separation of active transportation users.

Proposed cross-section features:

- **1 travel lane in each direction**
- “Traffic calming” design elements
- Wide inner boulevard for tree-planting, landscaping improvements
- Periodic, alternating side parking bay bulb-outs (~35 spaces total)
- 2m unidirectional **cycle tracks**
- 2m **sidewalks** on both sides
- Option for a half-height curb delineator or separated facilities



* Inner Boulevard width varies, 4m average

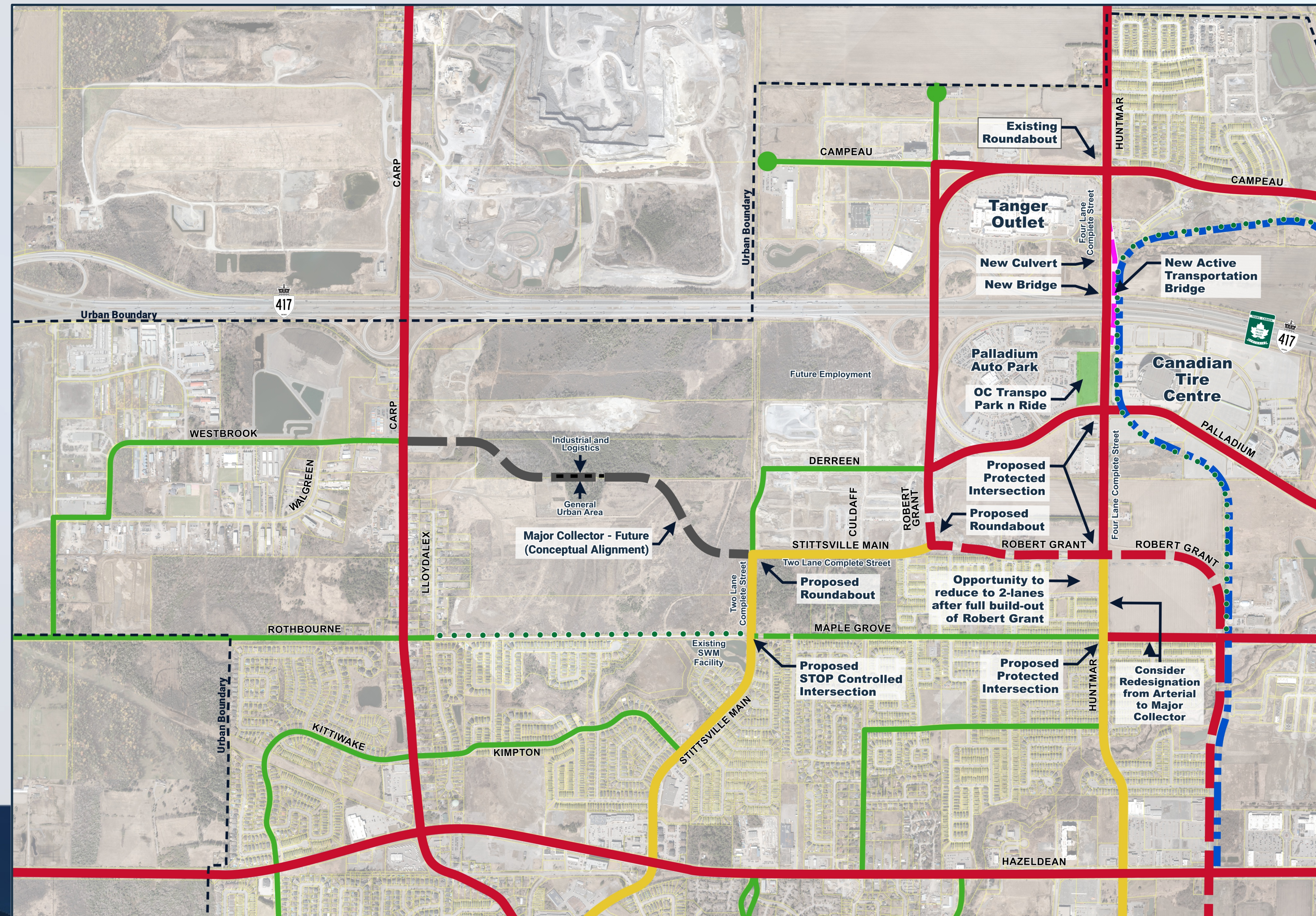
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Future Westerly Connection to Carp Road

There is an opportunity to plan for a future westerly extension of an east-west Major Collector from Stittsville Main Street to Carp Road.

Key planning considerations for the conceptual alignment includes:

- Overall network connectivity
- Support the New Official Plan and Urban Boundary Expansion
- Support the Transportation Master Plan Update
- Planned function of the entire road link between Carp Road and Robert Grant
- Integration with future development
- Property requirements
- Environmental effects to be considered (future study)
- Exact road alignment (future study)



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Elements of the Recommended Plan

Stormwater Management

- Stormwater management strategy for both roads will be developed in coordination with the adjacent developing communities.
- The Recommended Plan doesn't identify the need for stormwater facilities outside of the right-of-way.
- Both corridors have been designed with infrastructure that drains themselves through urban stormwater design including storm sewers and catch basins and conveyance to a common retention system prior to release.
- The urban cross section includes landscape open space to reduce runoff and provide water shedding for the adjacent pedestrian and cycling facilities.

Climate Change

In May of 2022 the City released their *Climate Change Vulnerability & Risk Assessment*. The purpose of this report was to identify the top climate risks facing Ottawa. The Recommended Plan includes consideration of climate change as design responses to each of the identified climate impacts. Some of these are described below.

Climate Hazard Priority Risk	Climate Change-Related Potential Impact	Design Response
 Extreme heat, drought and humidity	<ul style="list-style-type: none"> • Decreased use of active transportation systems • Increased health and safety risks for athletes, active recreation participants and active transportation users 	<ul style="list-style-type: none"> • Providing shade places including street trees, shelter structures • Providing rest and shelter areas • Planting plans, tree conservation reports
 Seasonal variability and change	<ul style="list-style-type: none"> • Damaged / compromised transportation systems from increased winter freeze-thaw 	<ul style="list-style-type: none"> • Proper sloping/crowning of facilities • Contemporary winter maintenance programs • Designed in consideration of subsurface materials; additional geotechnical investigation
 Increased precipitation volume and intensity		<ul style="list-style-type: none"> • Maximization of permeable surfaces, green landscaping boulevards • Stormwater facilities have been designed to be managed in accordance with municipal/provincial standards
 Extreme weather events	<ul style="list-style-type: none"> • Damaged / compromised road transportation or transit systems (road washout, bridge failures, landslides affecting roads, bridges, etc.) • Increased health and safety risks to users 	<ul style="list-style-type: none"> • Low-lying areas avoided • Utilization of City sewer design guidelines, Wet Weather Management Plan • Low impact development solutions part of stormwater management • Maximization of permeable surfaces, green landscaping boulevards
 Global climate change	<ul style="list-style-type: none"> • Increased material costs (e.g. asphalt) • Delayed projects • Increased cost of energy 	<ul style="list-style-type: none"> • Avoiding throw-away costs where possible. • Comprehensive consultation with identified stakeholders • Adhering to the municipal class EA process • Identifying future commitments required as part of the EA process

Huntmar Drive Widening (Campeau Drive to Maple Grove Road) and Stittsville Main Street Extension (Maple Grove Road to Robert Grant Avenue) Environmental Assessment (EA) Study

Project Impacts and Mitigation Measures

The Project will be designed and implemented with the benefit of contemporary planning, engineering, and environmental best practices and plans. Examples of these are listed below and will be updated and refined during the subsequent detailed design phase of the project.

- Real Estate Acquisitions when needed
- Stormwater Management Plan
- Public Communications Plan
- Construction Waste Management Plan
- Landscape Plan
- Environmental Management Plan
- Construction and Traffic Management Plan

Many of the anticipated impacts are temporary in nature (during construction). Monitoring measures include inspection and surveillance, and compliance monitoring for the above noted plans and complaint monitoring for such effects as noise and air quality (dust) during construction phases.

A noise and vibration impact assessment has been completed for each of the corridors based on the functional design. The assessment aligns with the municipal and provincial guidelines that apply to transportation projects.

Future vibrations associated with the long-term operation of the roadways are expected to fall below perceptible levels for existing sensitive receivers.

For noise, key criteria include:

- Noise sensitive receivers are identified as the rear or exposed side yard amenity areas of residential dwellings and other sensitive land uses.
- For residential dwellings, the noise sensitive location of concern is the outdoor amenity area located 3.0 m behind the rear wall of the dwelling, and 1.5 m above the ground.
- Where the forecasted noise levels at sensitive receivers are higher than 60 dBA or there's a change of 5dBA between existing and future conditions, noise mitigation is warranted.
- Where technically, administratively, and economically feasible, noise attenuation will be provided as part of the project. However, this 60 dBA threshold is not met for this project.
- To support future developments abutting the roadways, a condition of approval includes conducting noise assessment to determine if mitigation is required.

Based on the distance from the proposed roadway to existing sensitive receivers and the forecasted noise levels, noise attenuation is not required as part of this project.

Huntmar Drive Widening (Campeau Drive to Maple Grove Road) and Stittsville Main Street Extension (Maple Grove Road to Robert Grant Avenue) Environmental Assessment (EA) Study

Property Acquisition Processes

The implementation of the project will require property acquisition by the City. The details of property requirements will be confirmed at the time that the project undergoes detailed design, at some time in the future. The process of property acquisition will not begin until funding for the project is secured and design details are confirmed. The City of Ottawa will work directly with affected property owners throughout the subsequent process in accordance with the Real Property Acquisition Policy.

The City has the authority to acquire land through the direction and implementation of the City of Ottawa Official plan, as well as the *Municipal Act*, the *Expropriations Act*, the *City of Ottawa Act*, and the *Housing Development Act*. Key Aspects of the acquisition process include:

- **Dedication:** In some instances, the City may acquire property or property rights as a condition of the approval of development applications.
- **Negotiation:** This is the City's preferred method of obtaining property rights.
- **Expropriation:** This method is used, pursuant to applicable laws and regulations, when negotiations are not successful.
- **Current Market Value:** All real property acquisitions shall be supported with a current market value appraisal. This appraisal will be completed by an independent real estate professional or by a qualified City staff appraiser.
- **Highest and Best Use:** Appraisal reports will be based on the "highest and best use" of the property. Once complete, a report with full findings of the appraisal will be provided to the property owner.
- **Fairness:** The City's main objective is that landowners be treated fairly and compensated appropriately for the value of their property.

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Implementation and Phasing

The EA study recommendations allow for flexibility in phasing of project components. The implementation will depend on timing and funding availability.

Possible project phasing for **Huntmar Drive** can be described as follows:

There is an opportunity to address the critical need for an active transportation crossing of Highway 417 on the Huntmar Corridor by constructing an active transportation bridge crossing, as well as multi-use pathway approaches on both sides extending to the Cyclone Taylor intersection in the south, and north of Highway 417 in the north.

Based on the availability of funding and the rate of development progress, there is an opportunity to address the multi-modal transportation demand by widening Huntmar as a complete street from 2 to 4 lanes between Campeau Drive and Maple Grove Road. The widening will include a new complete street bridge, featuring contemporary pedestrian and cycling facilities over Highway 417.

Should funding become available for the Huntmar Drive widening before the construction of the potential active transportation bridge, then the complete street bridge may sufficiently address the need for an active transportation crossing of Highway 417, making the need for the active transportation bridge redundant.

For the section of Huntmar Drive between Robert Grant Avenue and Maple Grove Road, if Robert Grant Avenue has been built out to its ultimate 4-lane arrangement, and thus is able to accommodate the bulk of the north-south travel demand for the surrounding area, there may no longer be a need for Huntmar to be 4 lanes between Maple Grove Road and Robert Grant Avenue. In this scenario, this southern section of Huntmar may be reduced to a 2 lane arrangement as illustrated.

The **Stittsville Main Street** extension project should be constructed to address the project needs. However, it may be possible to construct it in sections, according to future travel demand, timing of the area's growth and development needs, and funding availability.

Huntmar Drive Widening (Campeau Drive to Maple Grove Road) and Stittsville Main Street Extension (Maple Grove Road to Robert Grant Avenue) Environmental Assessment (EA) Study

Next Steps

Following this opportunity for stakeholder input, the study team will review everyone's feedback and finalize the Recommended Plan and Functional Design and present it to Transportation Committee. Following Committee and Council approval, the Environmental Study Report will be finalized and posted for 30-day public review.

Please identify any comments or questions you would like to see addressed and provide those to the City by completing the survey on the project website.

Additional information on the project can be found on the City's website at: [Ottawa.ca/huntmardrivestittsvillemain](https://ottawa.ca/huntmardrivestittsvillemain)

You can also contact the City's project manager at:

Katarina Cvetkovic, P.Eng.
Senior Project Manager, Transportation Planning
Planning, Real Estate and Economic Development Department, City of Ottawa
Email: Katarina.Cvetkovic@ottawa.ca
Tel: 613-580-2424 ext. 22842

Your feedback is important to the success of this study.