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**Title :** **Traffic and Transit Management Plan**

**Client :** **City of Ottawa**

**Project :** **Trillium Line Extension Project**

*Prepared by :* Christopher Lyon, P. Eng., Director  
Transportation and Municipal, EDPM

Dan Chenier, Traffic Manager


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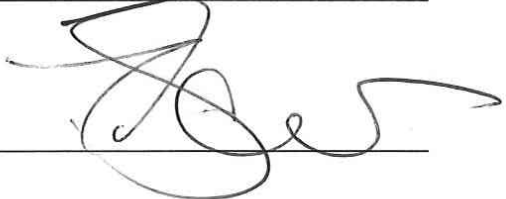

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*Reviewed by :* Cesar Palencia,  
Deputy Project Director


  


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*Approved by :* Ian Baker, Project Director



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Revision				Notes
Rev.	By	Appr.	Date	
00	M. Yang D. Chenier	I. Sanz	May 21, 2019	Initial TTMP Submission
01	M. Yang C. Lyon	C. Demetriades	Oct 10, 2019	TTMP Revision
02	C. Lyon	I.B.	March 15, 2021	TTMP Re-Submission

## Revision and Control

This Traffic and Transit Management Plan (TTMP) is a controlled document. Changes to this document will be implemented under controlled conditions and the most current issue of this document will be readily available to the user at all times. The TransitNEXT Traffic Manager is responsible for the control of this TTMP and will prepare any required changes, which will be reviewed and approved prior to issuing the updated document. Proposed revisions to the TTMP will be submitted to the City of Ottawa (the City)'s Entities in accordance with the Project Agreement (PA), Schedule 10 - Review Procedures. Any revisions to this document will be controlled and issued in the same way as the original document. The revision log shall be updated for all new revisions giving a brief description of the nature of the revision. In the case of major revisions, the revision log shall indicate that the revision is a "General Revision". Revisions will be marked with a line in the margin.

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**LIST OF APPENDICES**

Appendix A – TRAFFIC CONTROL CONCEPT PLANS

Appendix B – TRUCK INGRESS/EGRESS ROUTE PLAN

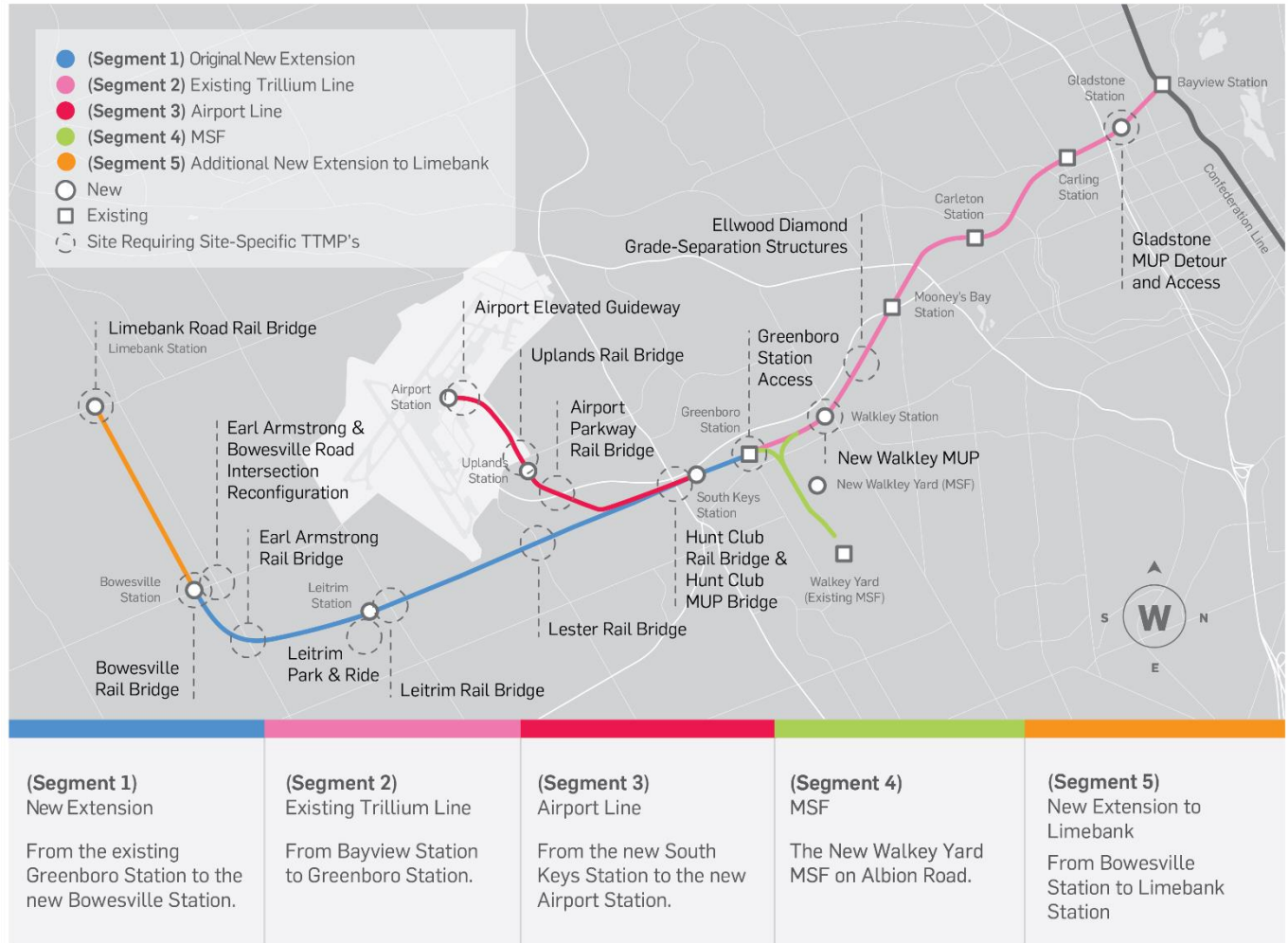
Appendix C – PROJECT SCHEDULES

Appendix D - CURRENT ROSTER OF TRILLIUM TRANSPORTATION SUB-STUDIES (LIVING DOCUMENT)




# 1 Traffic and Transit Management Scope

The Trillium Line Extension Project (the Project) primarily consists of an 11 km track extension south of Greenboro and a 4 km spur to the Ottawa MacDonald-Cartier International Airport, addition of 2 new stations on the existing line and 6 stations on the new extension lines, 8 rail-over-road bridge structures, 2 elevated guideway structures, and a new Walkley Yard Maintenance and Storage Facility (MSF). The project is divided into five segments as depicted in **Figure 1** below.



**Figure 1. Trillium Line Extension Project Segments and Prime S-TTMP's.**

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The Traffic and Transit Management scope is developed to maintain mobility for road users, including pedestrians, cyclists, vehicles, and bus transit, and that proper procedures are in place to mitigate construction impact to road users and adjacent residents and businesses. The purpose of this overall Traffic Transit Management Plan (TTMP) is to address the requirements of Project Agreement – Schedule 15-2 Part 7 – Traffic and Transit Management and Construction Access, which includes requirements on the following supporting sub-plans:


1. Traffic Control Plan;
2. Emergency Traffic Plan;
3. Traffic Incident Management Plan;
4. Traffic Management Implementation Plan;
5. Transit Management Plan;
6. Traffic Advisory Temporary Signage Plan;
7. Traffic Risk Assessment Plan;
8. Traffic and Transit Management Communications Plan;
9. Traffic and Transit Management Monitoring Plan;
10. Lane Closure Measurement and Verification Plan; and,
11. Property Access and Business Continuity Plan.

### Differentiation of Overall Project TTMP and Site Specific TTMP

As the term TTMP can be used ambiguously, it is beneficial to differentiate the Overall Project Traffic and Transit Management Plan and the Site Specific Traffic and Transit Management Plans as the O-TTMP and S-TTMP's respectively to facilitate better communication.

**O-TTMP:** This document, which serves as an overall guideline for the implementation of traffic and transit management measures and development of required documents – site specific TTMP's (S-TTMP's) comprised of relevant sub-plans described in **Section 5** of this document. This O-TTMP will be submitted for City of Ottawa (the City) review, approval, and updates as required.

**S-TTMP:** Detailed drawings and sub-plans developed for an individual construction site within the Project to address traffic and transit impacts caused by any project activities within or adjacent to the site. The development of these S-TTMP sub-plans will require consideration on site conditions, size of work area, existing traffic and transit operations, adjacent property use, proposed temporary traffic control, and site specific mitigation procedures to address impacts on stakeholders such as Emergency Management Services (EMS). An S-TTMP can include one or more stages for any individual site within the Project. For example, girder erection work on Hunt Club Rail Bridge for the two spans can occur on consecutive nights. These will be depicted as two stages but can be included in one S-TTMP package to minimize duplication of paper work and hence shortening the review process.

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## 2 General Approach


The O-TTMP is developed with the principle of respecting transit and road user expectations, such that users passing through a work zone or an operational transit interchange area will be able to find their way as safely and efficiently as possible. The S-TTMP development process and its execution should follow the general guidelines below:

1. **Maintaining Public and Worker Safety:** Safety is first priority when planning and executing our site activities. Wherever there may be a valid safety concern, construction strategies will be re-evaluated and adjusted to eliminate/reduce safety risks.
2. **Maintaining Pedestrian/Cyclist Mobility and Access:** Pedestrian access will be maintained at all times. Wherever pedestrian or MUP paths need to be closed, a detour within 20% of the original path length will be identified/constructed for pedestrians/cyclists. Maintain access to properties at all times as per Schedule 15-2, Part 7, Section 7.2 k) requirements.
3. **Maintaining Public Transit Operations and Mobility:** Bus transit operations will be maintained at existing operational levels or as decided by OC Transpo. Planning and implementation of bus detours and bus stop relocations will be coordinated with OC Transpo as required.
4. **Maintaining General Vehicular Traffic Mobility and Access:** Maintain capacity and level of service on existing roads by minimising duration and number of lane closures, or scheduling closures during low traffic volume periods. Maintain access to properties at all times as per Schedule 15-2, Part 7, Section 7.2 k) requirements.


Furthermore, the following guidelines and references will be used with respect to their application for the development of Traffic and Transit Management submittals in this project.

Table 1. Applicable Guidelines and Standards

Traffic Management Principles and Guidelines	Application
City of Ottawa Transportation Impact Assessment Guidelines	<ul style="list-style-type: none"> <li>▪ Approved travel analysis tools, methods, and parameters to assess temporary traffic control for road users</li> </ul>
OTM Books 1 to 6	<ul style="list-style-type: none"> <li>▪ Signage design principals</li> <li>▪ Sign design and patterns</li> <li>▪ Regulatory signs and warning signs</li> </ul>
OTM Book 7, Temporary Conditions	<ul style="list-style-type: none"> <li>▪ Principles of traffic control design for construction activities</li> <li>▪ Set up and application of traffic control devices used in construction</li> <li>▪ Typical layouts for control devices in various lane closure, diversion, and detour scenarios</li> </ul>
OTM Book 8, Guide and Information Signs	<ul style="list-style-type: none"> <li>▪ Guidance on developing guide and information signs to assist road users during road closures</li> </ul>

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OTM Book 11, Pavement, Hazard and Delineation Markings	<ul style="list-style-type: none"> <li>▪ Design and installation of markings and delineation on pavement</li> </ul>
OTM Book 12, Traffic Signals	<ul style="list-style-type: none"> <li>▪ Best practice guidelines for design and operation of traffic signals</li> </ul>
OTM Book 15, Pedestrian Crossing Facilities	<ul style="list-style-type: none"> <li>▪ Guidelines on the planning, design, and operation of pedestrian crossings</li> <li>▪ Legal requirements pertaining to pedestrian crossings</li> <li>▪ Guidance on recommended pedestrian crossing devices</li> </ul>
OTM Book 18, Cycling Facilities	<ul style="list-style-type: none"> <li>▪ Guidance on the development of temporary cycling facilities</li> <li>▪ Specifications for traffic signage to detour cyclists</li> </ul>
MTO Roadside Safety Manual	<ul style="list-style-type: none"> <li>▪ Guidelines for the treatment of roadside hazards and selection of appropriate protection systems</li> <li>▪ Guidelines on types of barrier systems and end treatments to be used for different applications</li> </ul>
OPSS and OPSD	<ul style="list-style-type: none"> <li>▪ Temporary Concrete Barrier and End Treatment Specifications</li> </ul>
Ottawa Accessibility Design Standards	<ul style="list-style-type: none"> <li>▪ Placement of transit shelters to allow easy wheelchair and scooter access</li> <li>▪ Provisions of traffic signal devices that suit the needs of seniors and people with disabilities</li> <li>▪ Requirements for pedestrian pathways</li> </ul>
Project Agreement, Schedule 15-2 Part 7 – Traffic and Transit Management and Construction Access	<ul style="list-style-type: none"> <li>▪ Project specific requirements</li> <li>▪ Requirement to avoid closures of consecutive intersecting streets to maintain mobility and access</li> <li>▪ Restrictive lane closure windows</li> <li>▪ Detour routes, lane shifts, and diversions according to OTM and City guidelines, with pavement condition requirements, barriers, and installation and removal of signs</li> <li>▪ Mitigation measures for drop-offs on roadways</li> <li>▪ Minimum lane widths through construction sites</li> </ul>
Project Agreement, Schedule 7 – Mobility Matters	<ul style="list-style-type: none"> <li>▪ Lane closure costs</li> <li>▪ Monthly Lane closure adjustment contribution calculation summary</li> <li>▪ Monthly Lane Closure Analysis Report</li> </ul>
Project Agreement, Schedule 15-2 Part 1, Article 5	<ul style="list-style-type: none"> <li>▪ Implementation Constraints</li> </ul>
Project Agreement, Schedule 15-2 Part 6	<ul style="list-style-type: none"> <li>▪ Connectivity Requirements</li> </ul>
OC Transpo, Interaction Zone Design Guidelines for Bus Stops and Bike Lanes	<ul style="list-style-type: none"> <li>▪ Interaction Zone Design Guidelines for Bus Stops and Bike Lanes</li> </ul>

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### 3 Site Specific Traffic and Transit Management Plans

#### Site Specific TTMP's Identified

TransitNEXT has completed a preliminary assessment on all construction sites and activities to minimize the traffic and transit impacts in compliance with Schedule 15-2 Part 7 closure restrictions. These restrictions are expressed under each roadway category below and their respective jurisdictions:

1. Existing Municipal Roadways – City of Ottawa;
2. Existing Transitway – OC Transpo;
3. Existing Airport Roadways – Ottawa MacDonald-Cartier International Airport Authority (OMCIAA); and
4. Existing Carleton University Roadways – Carleton University.

Sites of major construction impacts are identified in **Figure 1**, as a result of this assessment. Most of the traffic control measures required in this Project will accommodate construction activities at new bridge sites working from the road surface below. In addition, a number of other works including roadway modifications, transit route detours, MUP construction, intersection modifications and temporary relocation of at-grade rail crossings will require substantial lane and facility closures to facilitate these works.

The prime sites identified for potential construction-related impacts are listed in **Table 2** below with anticipated traffic impacts and mitigations. To minimize impacts to the travelling public, the most disruptive closures will be scheduled on weekends or overnight periods.

Table 2. Prime Sites and Traffic Impact and Mitigations

Location	Traffic Impact	Preferred Traffic Mitigation Approach
Walkley Road MUP construction	Widening of sidewalk into MUP requires long term lane closures	Any required lane closures or detours will be per OTM Book 7
Ellwood Diamond Grade Separation Structure over Transitway	Construction of bridge piers and erection of bridge girders and underdeck work will require closure of OC Transpo's Southeast Transitway	Schedule closure work at permitted closure windows. Coordinate with OC Transpo on closure times
Hunt Club Rail and MUP Structures	Erection of rail bridge and pedestrian bridge girders and underdeck work will impact vehicular, transit as well as pedestrian and cycling traffic	Erect girders during permitted night hours. Maintaining one lane in each direction and shifting traffic to one side
Lester Rail Structure	Erection of bridge girders and underdeck work will impact vehicular traffic	Lane closures with traffic control person to facilitate full intermittent closure during permitted hours (02:00-05:00)
NCR rail crossing Relocation at Lester Road	Track removal and installation work will require lane closure to accommodate	Intermittent, single lane closures to Lester Road are anticipated to provide for one bi-directional lane. Liaise with NCR Facility to ensure access continuity.


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Table 2. Prime Sites and Traffic Impact and Mitigations


Leitrim Rail Structure	Erection of bridge girders and underdeck work will affect vehicular traffic	Lane closures with traffic control person to facilitate full intermittent closure during permitted hours (02:00-05:00)
Earl Armstrong Rail Structure	Erection of bridge girders and underdeck work will affect vehicular traffic	Lane closures with traffic control person to facilitate full intermittent closure during permitted hours (02:00-05:00)
Earl Armstrong Road and Bowesville Road Intersection Reconfiguration	New pavement tie-in work to existing road will affect vehicular traffic	Traffic control people to direct traffic close to the existing Earl Armstrong/Bowesville intersection
Airport Parkway Rail Structure	Erection of bridge girders and underdeck work will affect vehicular traffic	Lane closures with traffic control person to facilitate full intermittent closure during permitted hours (02:00-05:00); Avoid EY Centre events; Liaise with OMCIAA
Uplands Rail Structure	Erection of bridge girders and underdeck work will affect vehicular traffic	Lane closures with traffic control person to facilitate full intermittent closure during permitted hours (02:00-05:00); Avoid EY Centre events
Airport Elevated Guideway	Erection of bridge girders and underdeck work will affect vehicular traffic on Airport Roadways	Lane closures with traffic control person to facilitate full intermittent closure during permitted hours (02:00-05:00); Liaise with OMCIAA
Bowesville Rail Structure	Erection of bridge girders and underdeck work will affect vehicular traffic	Lane closures with traffic control person to facilitate full intermittent closure during permitted hours (02:00-05:00)
Limebank Rail Structure	Erection of bridge girders and underdeck work will affect vehicular traffic	Lane closures with traffic control person to facilitate full intermittent closure during permitted hours (02:00-05:00)

The corresponding traffic control concept plans for these sites are shown in **Appendix A**. These plans are only partially complete and are used to demonstrate traffic reconfiguration and mitigation concepts. Detailed TCPs will be prepared to complete all required details as part of S-TTMP development process.

Lane closures on Municipal Roadways shown in these plans are incorporated in Lane Closure Target Letter submitted to the City at Financial Closure of this Project. Other minor traffic control expected for this project are associated with site investigations, access/egress control, and utility relocation. These are not anticipated to cause major traffic impacts.

### Truck Ingress/Egress Routes

Heavy trucks will be placed on haul routes for delivery of material and equipment to work sites and removal of construction waste. Truck Ingress/Egress routes will be determined based on truck size and volumes and in

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conjunction with City of Ottawa’s Truck Route Maps to comply with load restrictions, and in coordination with the City Traffic Representatives. Wherever there may be operational safety concerns from traffic movement conflicts, traffic control persons or Ottawa Police Services will be used for safe access control.

At preliminary assessment, a Truck Ingress/Egress Route Plan is shown in **Appendix B**. This plan identifies existing City truck routes on near project and construction laydown areas within project properties. S-TTMPs will show detail of construction access points and required traffic control. Permits for oversize vehicles from the City of Ottawa and Ministry of Transportation of Ontario will be obtained as required. Truck Ingress/Egress Route Plan may be revised upon further assessment.

**Pedestrian Access Plan / Construction Site Pedestrian Control Plan**

The standard adopted for situations where construction vehicles must cross a MUP includes stop controls for vehicles and flaggers to safeguard cyclists and pedestrians during vehicle movements.


Maintaining Existing Access to Properties: Consistent with PA, Part 7, effective access will be maintained to all properties. Irrespective of time of day. This provides for front door pedestrian access, which will be maintained at all times unless impossible, and when impossible to maintain, notice will be provided to the City and the property owner a minimum of 60 days in advance of closure. When loss of access is agreeable to the property owner, the City may provide an exception.

**4 Project Schedule**

At the time of preparation of this Traffic and Transit Management Plan (TTMP), the detailed schedule for the construction of the bridges was under development and a detailed schedule will be provided in future update of this document. A preliminary Work Schedule – Time Chainage Diagram is attached as **Appendix C** as a reference in this submission. An overall table format schedule (“2021 Project Overview”), a shorter-term table-format lookahead, and a sample SSTTMP schedule are attached in **Appendix C** as references. These schedules are included as samples only; the SSTTMP-style schedule is only included in specific SSTTMPs.

The TTMP team will be extracting dates from the project schedule with the confirmation from the construction team to plan for any Traffic and Transit Management tasks. The project schedule is critical to the development of target lane closures and its monthly updates for the calculation of lane closure adjustment contribution as per Schedule 7- Mobility Matters.



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## 5 TTMP Sub-Plans

The TTMP sub-plans below describes the approach and processes required to assemble a complete S-TTMP package. Together, they verify all traffic control measures are proposed with reasonable consideration for road users, stakeholders, and adjacent property owners. Each plan addresses the processes used to develop the individual S-TTMP sub-plans for site implementation to achieve consistent quality results. They are also tailored to the project scope to streamline the processes and procedures in order provide clear and practical guidance to TransitNEXT's Traffic and Transit Management Team.

### 5.1 TRAFFIC CONTROL PLAN

Development of a Traffic Control Plan (TCP) will be the starting point in the S-TTMP process. TCPs are to detail all proposed set-up of traffic control devices and traffic operational changes to accommodate construction activities.


The Traffic and Transit Management team will develop TCPs in consultation with the City and other affected stakeholders such as OC Transpo and adjacent property owners. The Traffic and Transit Manager will address all comments from these stakeholders in the TCP revisions for final approval prior to field implementation. The TCPs will be designed to applicable standards and with good engineering judgement, with an aim to maintaining safety for road users and construction workers as well as minimizing the Project's impact on pedestrians, cyclists, buses, trucks and other vehicular traffic. These standards include but may not be limited to:

- Schedule 15-2, Part 7 of the Project Agreement
- Ontario Traffic Manuals (OTM Books), 2014
- Ministry of Transportation of Ontario (MTO) Roadside Safety Manual, 1993
- Ottawa Accessibility Design Standards, 2012

The TCPs should show the existing field conditions in the background and reflect all proposed field modifications and additional traffic control devices and measures such as signage, Portable variable message signs (PVMS), temporary pavement markings, temporary barriers, delineators, traffic control people, and police. Furthermore, the TCPs should show the following information as per the requirement in Schedule 15-2, Part 7 of the PA:

- a) Work area locations
- b) Existing conditions (include critical signage)
- c) Noting site condition/arrangement subsequent to traffic control set-up (reinstate to existing condition or implement to next traffic control stage).
- d) Construction ingress/egress points
- e) Temporary signs
- f) Traffic directions
- g) Lane configuration in the work zone including lane widths
- h) Lane use requirements
- i) Access/entrances being impacted and mitigations to maintain residential/business access
- j) Anticipated schedule of work including hours

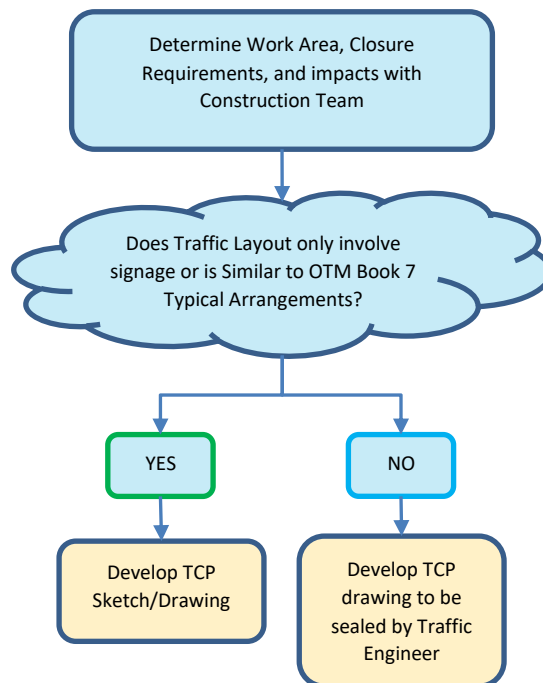



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- k) Pedestrian, cyclist, or traffic detours, including where construction access routes cross a sidewalk, pathway, MUP or cycling facility identified in a Council-approved Plan or policy.
- l) Changes to traffic signals
- m) Posted speeds on affected roads
- n) Location of existing bus stops and any proposed relocation
- o) Transit detours
- p) On-street parking spaces affected by the work and any compensation measures to maintain the street’s parking capacity wherever applicable

For practicality, it is necessary to distinguish two types of TCPs. The first are standard TCPs that adhere OTM Book 7 guidelines and are similar to applicable typical layouts. These plans will be sketched in the software Invarion (By RapidPlan) or AutoCAD, showing all relevant information as listed above. Additionally, typical pedestrian signage plan or MUP detour plans can be developed as sketches with reference to Ontario signage designation labels. These plans do not require an Ontario Professional Engineer’s seal.

The second type are custom TCPs with deviation from typical layouts on OTM Book 7 due to more complicated site conditions that will require special adjustment for road user expectations and safety. Such custom TCPs will require a Professional Engineers of Ontario (PEO) Engineer’s seal. Additionally, where temporary concrete barriers are required for protection of workers within the clear zone of a road, a TCP plan with design of temporary concrete barrier design and end treatment will be developed. This will be stamped and sealed by an Ontario Professional Engineer prior to submission to the City for final review and approval as per review procedures in Schedule 10. TCPs will be updated as required to the satisfaction of the City for final approval prior to site implementation.



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**Figure 2. Traffic Control Plan Development Flow Chart**

It is not anticipated that Traffic Analysis for temporary conditions will be required to model the TCPs in this Project, as the duration of closures are expected to be short and limited to off peak and night hours. No long term lane closures over 7 days are expected at the time of this O-TTMP preparation. However, should peak hour closures become required, traffic analysis for temporary conditions will be provided.

The TCP will clearly indicate physical impacts on bus shelters and stops to start coordination with OC Transpo in accordance with Schedule 15-2 Part 7 requirements. Refer to the following Transit Management Plan in **Section 5.5** of this document for further details on Transit Management Plan.

## **5.2 EMERGENCY TRAFFIC PLAN**

The primary objective of the development of the Emergency Traffic Plan is to document TransitNEXT’s process and procedures to be used to maintain and facilitate emergency vehicle access to and through the affected site where obstructions may be introduced. The Emergency Traffic Plan specifies how the movement of emergency service vehicles will be accommodated such that the works do not prevent the achievement of target response times by fire, police and ambulance crews.


The general approach to facilitate emergency access is always to maintain an obstacle-free passageway within the construction work area on roadways. Wherever this is not achievable, detours should be determined in consultation with Emergency Service Providers. Of particular importance, during full closure of roadways at bridge sites, a detour around the site during the permitted full closure hours will be required as it may not be possible for construction operations to immediately cease to allow safe passage for Emergency Responders. In such case, a detour will be defined in collaboration with all Emergency Service Providers. This will be documented in the Emergency Traffic Sub-plan within the S-TTMP. Notification will be sent to emergency service providers before the implementation of the work. Emergency Service Provider Representatives will be invited to our pre-construction meetings to address any other emergency traffic access concerns.

## **5.3 TRAFFIC INCIDENT MANAGEMENT PLAN**

The Traffic Incident Management Plan (TIMP) facilitates efficient and effective response to incidents affecting the travelling public with an overall objective of clearing incidents in the minimum time possible. Our primary obligation is to facilitate safe and effective transportation operations for each mode (i.e., pedestrians, cyclists, transit vehicles and private vehicles), in and near the construction sites.

Construction sites will be regularly monitored by our on-site Supervisors and through City traffic cameras. In the event of an incident, Emergency Management Services (EMS) and/or the City of Ottawa Traffic Control Centre will be contacted to mitigate identified issues. The Site Supervisors will work proactively with the City and other relevant Stakeholders to clear any incidents as soon as possible to resume normal traffic operations.

TransitNEXT’s TIMP will be consistent with the City’s Emergency Management Plan. TransitNEXT will protect both on-scene responders and the travelling public, reduce delays and associated impacts on travelers and the possibility of secondary incidents, and quickly release response resources servicing incidents back to regular service. The core element needed for successful management of emergency incidents is definition of response

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protocols. Each group (TransitNEXT, emergency responders, City traffic staff, MTO) must understand their responsibilities in the event of an incident.

In order to successfully achieve this, it is recommended to refer to the City’s Traffic Incident Management Group. In addition, it is recommended that TransitNEXT’s Traffic Manager attend meetings from time-to-time and liaise directly with the City’s Incident Management Coordinator prior to major construction components impacting traffic.

### Traffic Incident Notification and Reporting Process

TransitNEXT’s Traffic Manager and the City’s Traffic Representatives are to be advised when an incident occurs immediately after the Emergency Services (if required). In some cases, TransitNEXT’s site superintendent may be the first to arrive at the scene of the incident. Emergency services (911) should be contacted immediately if there is danger to life, bodily harm, or property from a motor vehicle accident, fire, gas leak, etc. The site superintendent will make an immediate decision on the first course of action and provide for the safety of the public by contacting emergency services and assisting the first responders where appropriate, while cognizant of the need for professionals to manage the overall situation / on-site conditions. Traffic incident reports, depending on the impact on traffic, may be discussed at Traffic Management Committee (TMC) meetings and should be reported to the City’s Traffic Incident Management Group.

Following the process noted above, the City will be advised by e-mail followed by a telephone call to the City Officer on Duty outside City work hours. During work hours, the City’s Transit and Traffic Management Lead will be the contact for TransitNEXT.

The next course of action by the on-site supervisor is to assist where appropriate and mitigate traffic disruptions and impacts. TransitNEXT’s Traffic Manager and other Site Supervisors will be made available to assist the on-site Supervisor and collaborate with and City of Ottawa Traffic Management Services.

Once the incident has been cleared, the Site Supervisor will document the incident through the **Traffic Incident Notification Report**. This report will identify the type of incident, summarize it, list the potential contributing factors or probable causes associated with it (in the opinion of the Site Supervisor), and identify the next actions required from TransitNEXT and the City.

The Traffic Incident Notification Report is then filed and submitted through Document Control to the City within 48 hours of the incident. When and where appropriate, the incident will be raised to the TMC for review as part of the overall process. A determination will be made following this review regarding modifications and applicable changes will be implemented via the TCP process.

The notification and reporting process is explained above and summarized in the Flow Chart on **Figure 3**.

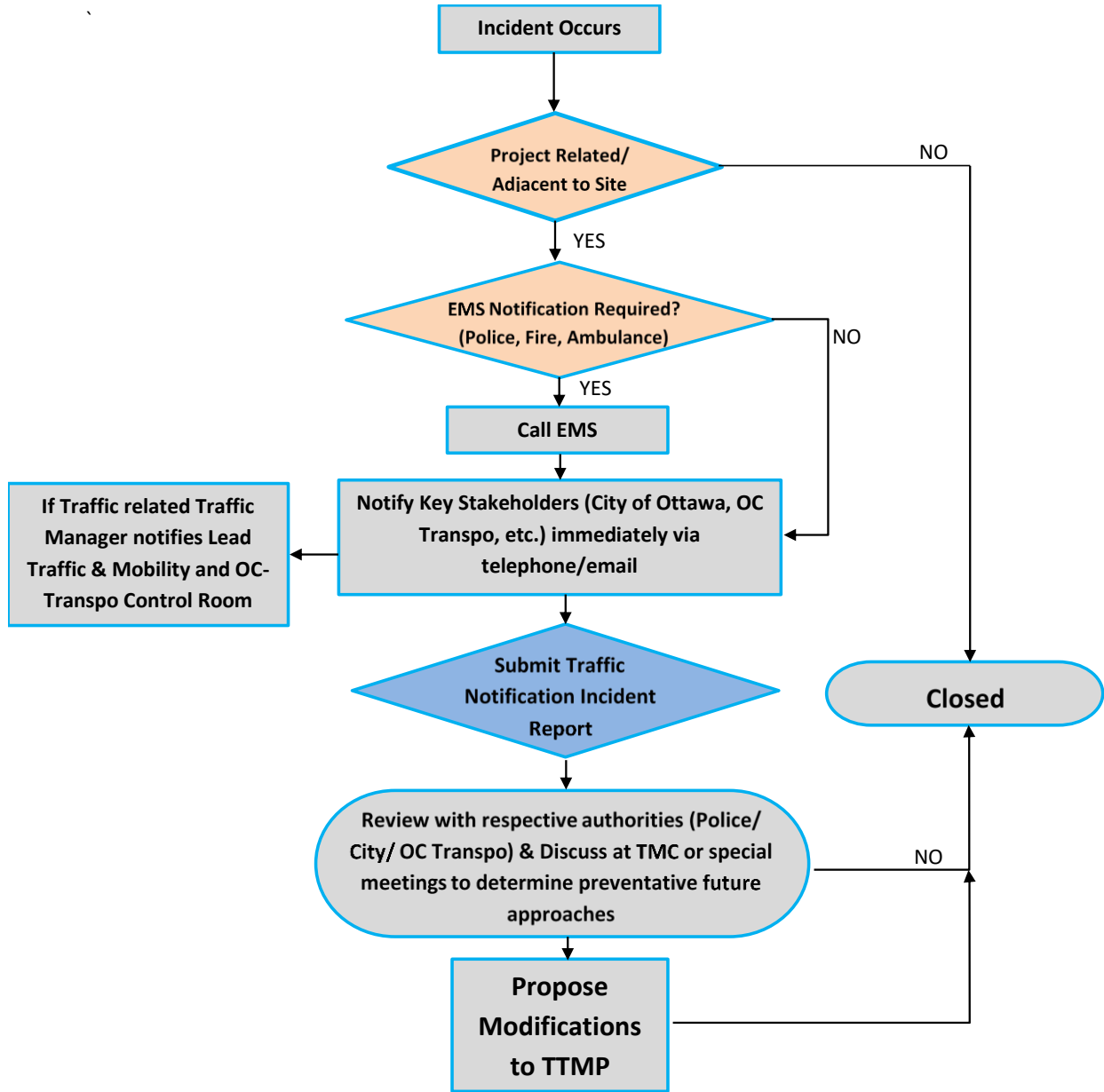



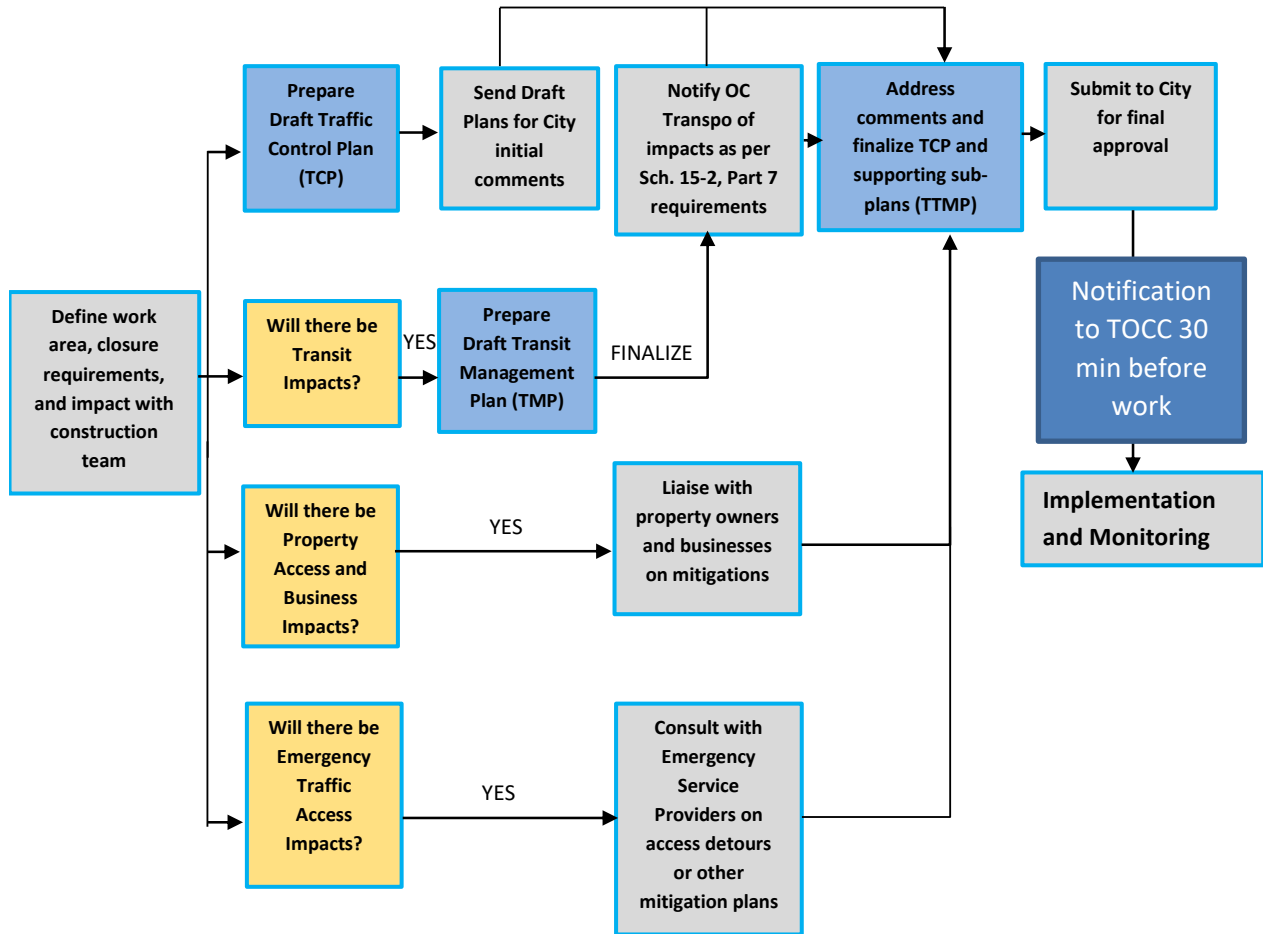
Figure 3. Incident Management Flow Chart

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## 5.4 TRAFFIC MANAGEMENT IMPLEMENTATION PLAN

The Traffic Management Implementation Plan is used to identify the on and off-site staffing requirements, define processes to confirm that the TCP and other supportive sub-plans in the S-TTMP are developed and implemented efficiently.

When defining the scope for an S-TTMP, traffic control requirements must be communicated from the construction team to the traffic management team. Requirements such as work area size, number of road closures and their duration, as well as impacts to any road users and stakeholders will need to be documented for the development of a draft TCP. As impacts to transit, property access, and emergency traffic are identified as a result of proposed traffic control measures, corresponding sub-plans will also be developed to address the impacts and document any mitigation methods as part of the S-TTMP in liaison with the affected stakeholders. It is acknowledged that OC Transpo will require significant notice to review and perform their own assessment to bus service operations and facilities. TransitNEXT's construction management team are advised of the notification requirements when developing construction plans and schedules so transit impacts will be identified at early stages to allow enough time for the preparation of Transit Management Plans. Once all relevant sub-plans are developed, the S-TTMP will be submitted for final approval by the City prior to site implementation and monitoring. This process is summarized in flow chart **Figure 4** below. The development of all other sub-plans is described this Section 5.




**Figure 4. Site Specific TTMP (S-TTMP) Process Flow Chart**

TransitNEXT’s Traffic group will be responsible for the duties outlined in Schedule 15-2 Part 7 Article 7.3 and will respond to traffic control requirements which will support construction activities. They will direct all traffic control work on site and liaise with the City, OC Transpo, Ottawa Airport Authority and Carleton University Emergency Management Services personnel as needed. They will record the duration of all closures, observation on traffic delays, monitor traffic and transit operations and bring any issues to the attention of the Traffic Manager and Traffic Management Committee (TMC) as prescribed by the Project Agreement.

The TransitNEXT’s Traffic and Transit Management Team will be lead by Traffic Manager – **Daniel (Dan) Chenier**, who will be responsible for the following:

- Development, implementation, and management of the S-TTMP’s;

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- Ensuring the City is kept informed of all upcoming traffic activities and any revisions of the TTMP;
- Coordinate with persons/parties carrying out Work in areas adjacent to the Project;
- Co-chairing with the City the Traffic Management Committee;
- Ensuring the timing and organized delivery of public and stakeholders communication information;
- Participating as TransitNEXT’s representative in coordinating with the City’s Traffic Demand Management program and the Traffic Incident Management Group;
- Sign-off of each and every S-TTMP and TTMP sub-plan prior to their submission to the City;
- Review and sign-off the Lane Closure Analysis Report required in Schedule 7 – Mobility Matters; and
- Attend the Mobility Matters Review Meetings described in Schedule 7 – Mobility Matters.

The Traffic Manager will acquire traffic design services from qualified Traffic Engineers whenever necessary to meet the requirements of the S-TTMP. The Traffic Engineer will attend the Traffic Committee Meeting whenever required.

TransitNEXT has retained services from **Beacon Lite** to implement traffic control set-ups on site. Beacon Lite has over 50 years of experience in Traffic Control business. They are experts in traffic safety and have well trained and fully qualified local staff, who can execute the job safely and efficiently. They will be on site full-time when construction activities are underway, responsible for monitoring activities as required, and provide the relevant Daily Traffic Monitoring and Lane Closure reports.


The Traffic Manager will coordinate Beacon Lite Traffic Supervisors for the timely implementation and monitoring of traffic activities within the Project. Collaboratively with City and OC Transpo staff, they will monitor traffic operations and adjust construction provisions as required. They will also work proactively as each new Works phase approaches, to put plans in place to transition to the next phase in terms of detours, signage, pavement markings, signals, PVMS installations, and information sent out to Stakeholders and the public.

## 5.5 TRANSIT MANAGEMENT PLAN

Transit impacts anticipated for this project pertain to any work that impacts Transit services. The existing transitway system referred to in this project is limited to the Southeast Transitway, from Billings Bridge Station to South Keys Station, and the Leitrim Park & Ride Station and Facility. The requirements for mitigation of impact on these services are stipulated in Schedule 15-2 Part 7, Article 3 and elsewhere in Part 7. Whenever applicable to the Project, the Traffic and Transit Management team will submit a Transit Management Plan as a sub-plan to any S-TTMP to depict how impact on bus services will be mitigated.

The Traffic Manager will consult the City and OC Transpo on any proposed changes to transit services and apply the following strategies during construction to make such proposed changes acceptable to the City and OC Transpo:

- Prioritize transit user safety and construction worker safety
- Maintain existing bus routes and frequency of service as much as possible
- Minimize the need for access to, and working within the Transitway

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- Maximize work within permitted closure windows during off peak hours, night hours, and weekend hours
- If bus stop relocations are required, minimize walking distance to temporary bus stops
- Minimize travel time for pedestrian access to existing Transit facilities
- If bus detours are required, maintain the existing route as much as possible and minimize travel time variability
- Minimize additional bus traffic in residential areas
- Give buses priority to pass through construction zones during flagging operations

The Transit and Traffic Management team works closely with our design and construction teams to develop construction methodologies to eliminate or minimize impacts to transit services throughout the construction of the Project. In compliance with OC Transpo’s restrictive closure windows and other operational constraints stipulated in Schedule 15-2, Part 7, we have identified the following transit impacts from our construction activities, these are tabulated in **Table 3** below:

**Table 3: Transit Impacts Anticipated in Project**

<b>Transit Impact</b>	<b>Applicable Sites</b>	<b>Type of Work</b>
Bus stop relocations	South Keys Transitway Station	Bus Bay and Shelter Construction
	Hunt Club Road	Bridge work
	Leitrim Park & Ride	Park & Ride Expansion
	Walkley Transitway Station	New MUP construction
Bus route detours	Hunt Club Road	Bridge work
Transitway lane closures	Transitway at Ellwood Diamond Grade Separation	Bridge demolition and new bridge work
	South Keys Station	Bus bay construction
Traffic Stoppage with potential bus service delays	Lester Road Leitrim Road Uplands Road Airport Parkway Paul Benoit Drive	Bridge work

The Traffic and Transit Management team should maintain existing local and rapid transit services as closely as possible. No major bus service disruptions are anticipated for the execution of this project. Nevertheless, the Traffic and Transit Management team will coordinate with the City and OC Transpo for any proposed route changes that may be required during the execution phase of the Project following the transit route schedule dates shown in **Table 4** below.




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Table 4: Tentative Transit Route Schedule Change Dates

YEAR	APRIL	JUNE	SEPTEMBER	DECEMBER
2019		23	1	22
2020	19	21	6	20
2021	18	20	5	19
2022	17	26	4	25
2023	23	25	3	24


Notices with an explanation of construction activities will be submitted to the City’s Traffic and Mobility Lead and designated OC Transpo representative in accordance with the notification requirements in the **Table 5** below.

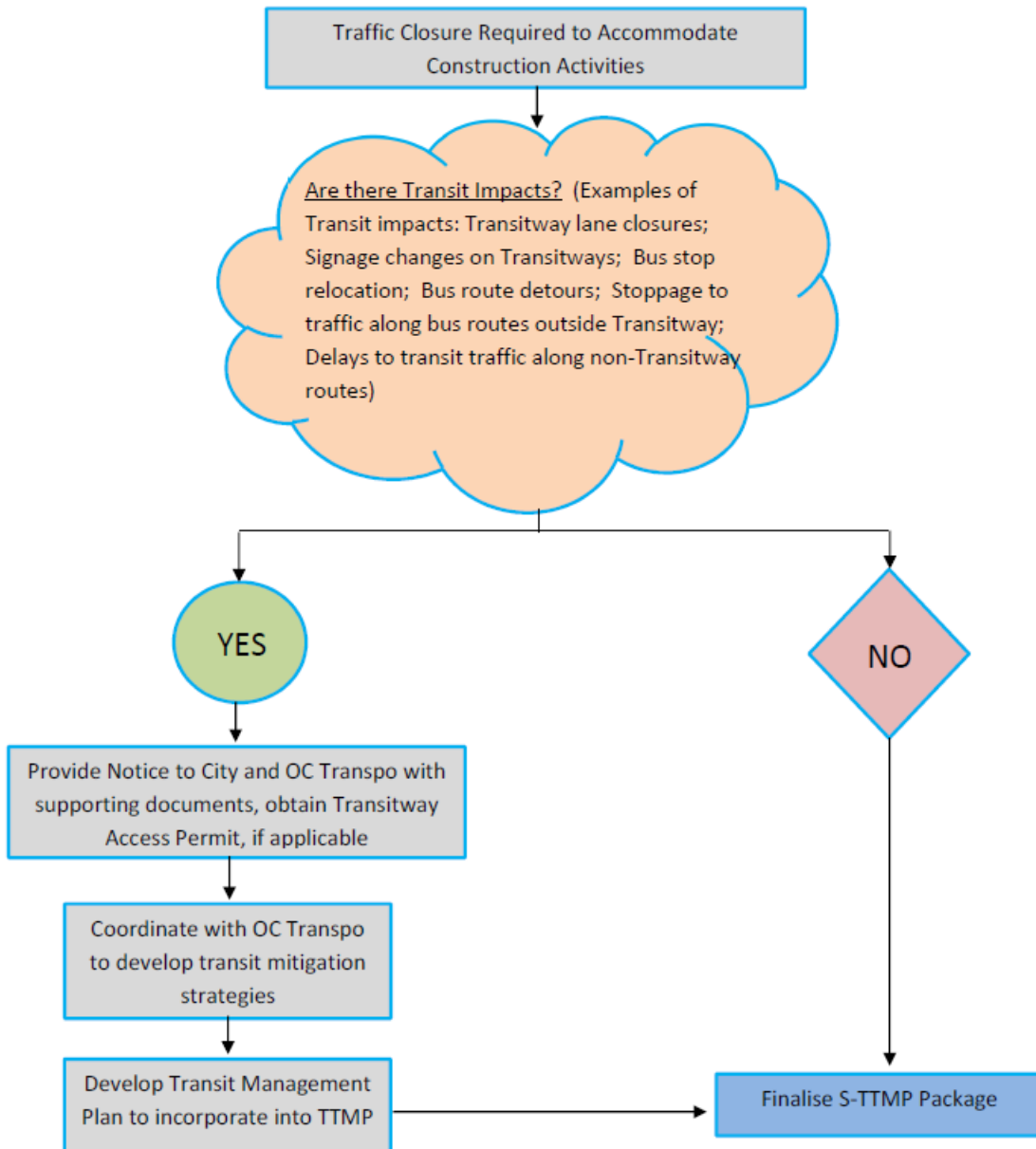
Table 5: Notices Required by OC Transpo

Impact on Transitway	Notice Required
Working in or adjacent to transitway lanes	5 business days to the City and OC Transpo. Notice to OC Transpo Control Centre 30 minutes prior to set-up
Change in signage	15 business days to the City and OC Transpo
Bus stop relocation	20 business days to the City and OC Transpo
Access to Transitway	35 business days to the City and OC Transpo
Lane closures to bus circulation roadways	35 business days to the City and OC Transpo
Lane closures on transitway, bus detours that requires additional travel time	6 months to the City and OC Transpo

#### Development of Transit Management Plan as Part of S-TTMP


Upon request on traffic control involving transit impacts, supporting documents such as draft TCPs and sketches on transit mitigation will be immediately prepared for the City and OC Transpo review in compliance with notice requirements as discussed above. A technical traffic management meeting will be arranged to discuss transit mitigation strategies. Once a mutually agreed solution is developed, it is to be documented in the Transit Management Plan as part of the S-TTMP for the construction activity. This process is summarized in **Figure 5** below. Moreover, application for Transitway Access Permits will also be submitted along with the notification to OC Transpo for approval with TCP and other supplementary plans necessary for OC Transpo’s approval.

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**Figure 5. Transit Management Plan Development as Part of S-TTMP.**

Transit Management Plans will be updated as required to the satisfaction of the City and revisions will be submitted to the City for final approval prior to site implementation.

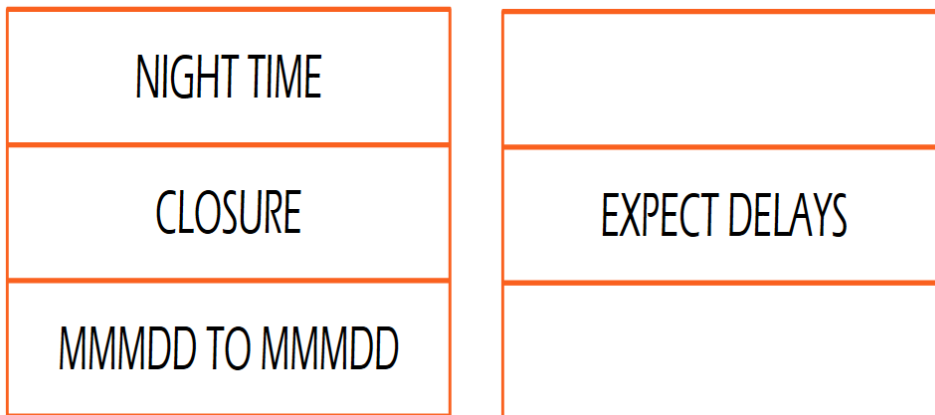
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## 5.6 TRAFFIC ADVISORY TEMPORARY SIGNAGE PLAN

The construction activities causing major traffic impacts are scheduled within the nighttime or weekend hours to minimize delay and inconvenience to road users. These only require short term traffic control set-ups like the typical layouts on OTM Book 7. As such, we will only use advisory temporary signage on a few locations where full closures are required for girder erection and underdeck work. Since most of these major works will be carried out at night, we will erect PVMSs one week prior to the construction activities requiring full closure of a road with traffic control people to give road users advanced notice of the expected delays.


Nighttime intermittent full closures for bridge work are anticipated at the following locations:

- Airport Parkway
- Uplands Road
- Hunt Club Road
- Lester Road
- Leitrim Road
- Earl Armstrong Road
- Bowesville Road
- Limebank Road



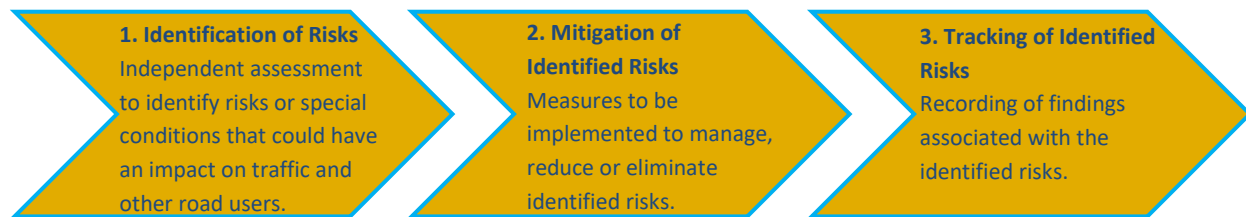
**Figure 6. Sample Advisory PVMS Message Prior to Nighttime Full Closures for Bridge Work**

All PVMS messages and information signage will be bilingual as per Schedule 15-2 Part 7 Article 1.5. Wording used on PVMS will be provided or approved by the City. The Traffic and Transit Management team will submit a Traffic Advisory Temporary Signage Plan separate from the TCP as part of the S-TTMP for the individual sites above. These signage plans, or revisions thereof must be accepted by the City prior to implementation on site.

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## 5.7 TRAFFIC RISK ASSESSMENT PLAN

By changing all proposed road-over-rail structures to rail-over-road structures, we have simplified the traffic control requirements, thereby eliminating and reducing many traffic-related risks. Most of the traffic control required in this project will be limited to short duration (<24 hrs) lane closures compliant to the OTM Book 7 guidelines. The following **Figure 7** shows the general traffic risks assessment process that will be applied in this Project.




**Figure 7. Traffic Risk Assessment Process**

Following this approach, a list of traffic related risks and mitigation methodologies are developed in **Table 6** below.

**Table 6: Traffic Risks and Mitigations**

Risk	Mitigation
Worker Safety adjacent to road – vehicle veering off into construction zones	Reduce posted road speed Use temporary concrete barriers (TCBs) and energy attenuators as necessary Educate workers on working safely adjacent to traffic Use police services and surveillance
Introduction of TCBs and end treatments – vehicular collision with end treatments	Install reflective hazard signage Install barriers and end treatment as far away from the lane edge as possible
Construction accessing/egressing from sites conflicting with traffic flow	Have construction vehicles give general traffic the right of way Provide clear construction limit signage Reduce road speed Erect speeding fine signs within construction limits Allow only right-in and right-out movements for construction vehicle ingress/egress
Dust clouds generated at construction ingress/egress points will obstruct driver’s views and sightlines	Have construction vehicles use paved shoulders for smooth acceleration and deceleration; Control dust with water sprays

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Girder lifting operation over traffic – girder dropping over live lanes	Stop all traffic during girder lifting Only allow traffic flow when nothing is overhead and crane masts are angled away from the traffic footprint
Concrete pour on bridge deck over traffic – wet concrete dropping over live lanes	Stop all traffic during concrete pouring Suspend a catchment cloth under bridge during deck pour
Emergency response delay through site	Allow sufficient space to bypass the construction work zone on pavement at all times – refer to Emergency Traffic Plan

An independent safety audit can be arranged if the proposed traffic control measures by the Traffic and Transit Management Team are deemed insufficient to address any specific traffic risks by the City. A Road Safety Audit team will be selected and their qualifications will be approved by the City prior to the performance of this work.

The Traffic and Transit Management Team will submit a Traffic Risk Assessment Plan as part of the site-specific TTMPs for the construction sites with traffic impacts for City’s review and approval in accordance with Schedule 10 – Review Procedure. TransitNEXT will implement a traffic control setup on site upon TTMP acceptance by the City.

**5.8 TRAFFIC AND TRANSIT MANAGEMENT COMMUNICATIONS PLAN**

TransitNEXT Traffic team will provide information to our Communications team for their preparation of notifications about upcoming closures and detours for the City’s content approval. These will be provided based on the lead times established in the Schedule for works that are anticipated to have a major, moderate, or minor impact. These 3 levels of impact are defined on Schedule 18 and are summarized here:


**Major Impact** shall include but not be limited to overnight construction or maintenance, temporary construction activities, paving, commissioning activities, privately owned elements to be relocated or removed by TransitNEXT, and transit stop relocations.

**Medium Impact** shall include but not be limited to major intersection work, any disruption to water, gas and/or other utilities, and impacts from noise or dust. For clarity, the notifications will be delivered by TransitNEXT on behalf of the City to all affected properties and in consultation with Utility Companies, as applicable.

**Minor Impact** shall include short-term lane closures, minor pedestrian detours, and access and driveway work. TransitNEXT will ensure that access is maintained to properties impacted by construction at all times.

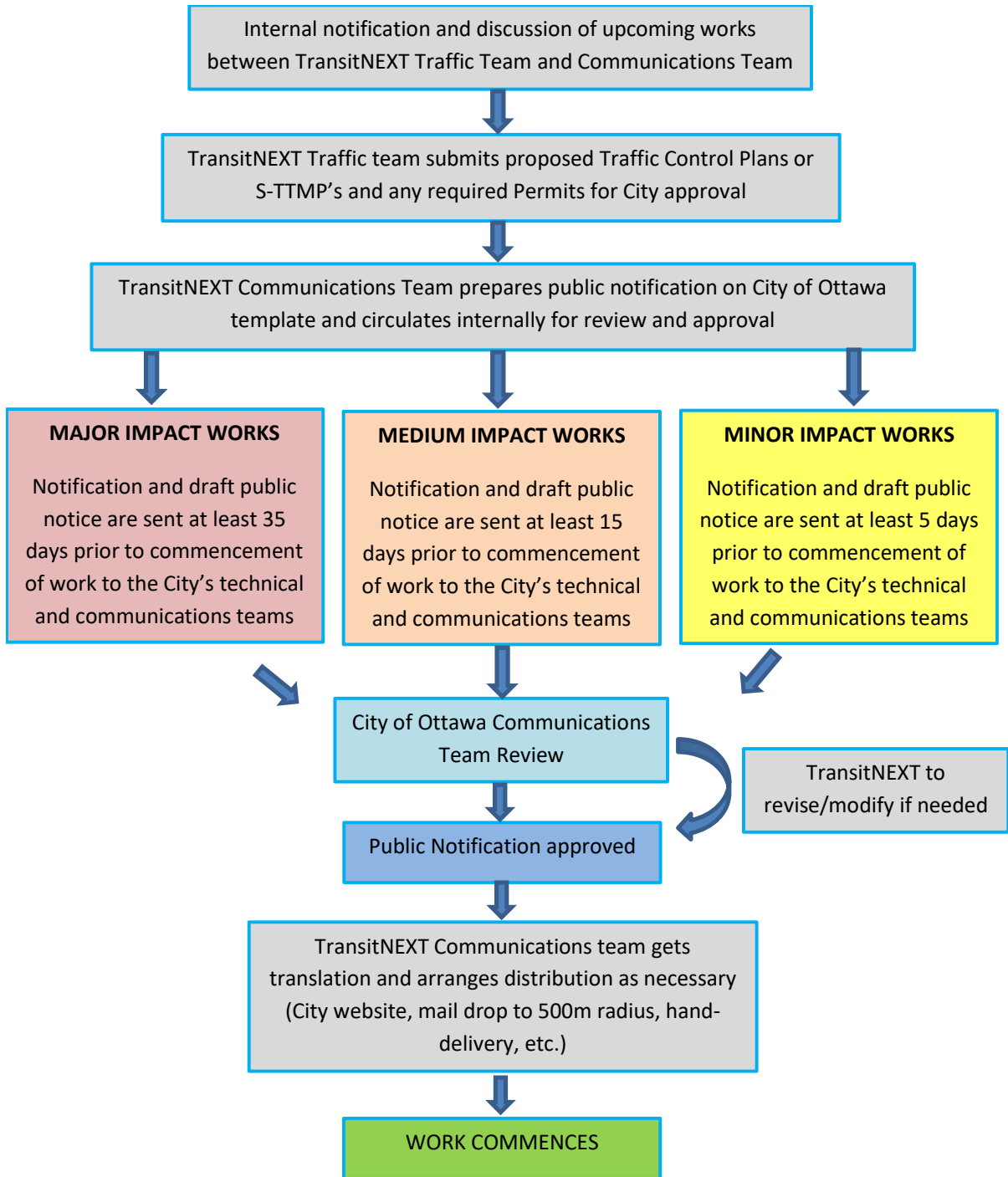
Traffic and Transit impact notifications will support the official notification made to the City by TransitNEXT’s construction staff. Communications notifications provide information to public on traffic impacts that will be supported by graphical representations of TransitNEXT’s traffic control plans or other S-TTMP sub-plans as approved by the City. The notification details include duration and location of closures or detours, supported by drawings and descriptions of temporary facilities and measures with appropriate detail to allow the City to:

- Communicate anticipated impacts of closures and detours to commuters, residents, and affected business owners
- Facilitate meetings with individual stakeholders affected at specific locations during construction, as required


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- Populate the City’s traffic report and its traffic account on Twitter
- Address inquiries to ServiceOttawa, 311, lightrail@ottawa.ca, OC Transpo, and city councillors’ offices
- Support planning, development, and placement of notices and advertising related to the works

**Figure 8** below demonstrate the process required to produce public notifications for any traffic/transit impacts caused by construction.



**Figure 8. Traffic and Transit Public Notification Flow Chart**

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## 5.9 TRAFFIC AND TRANSIT MANAGEMENT MONITORING PLAN

For continuously improving traffic control design and implementation strategies to benefit public safety and mobility, the Traffic Control Supervisor will monitor the site whenever traffic control measures are implemented, making immediate adjustments to address safety concerns and noting instances where the designed measures are not meeting the expectations of the TTMP and its sub-plans. These findings will be documented in daily reports and site records, so the Traffic and Transit Management Team can assess the current traffic control practice and identify the remedial action required to meet the expectations of the traffic control set out in the TTMP and its sub-plans.

The Traffic Control Supervisor will produce the following daily reports tailored to the scope of this project:

- Daily Traffic Management Site Records
- Daily Traffic Monitoring Reports
- Daily Lane Closure Reports

### Daily Traffic Management Site Records

The Traffic Control Supervisor will bring a copy of the most up-to-date S-TTMP to each site and validate that the traffic control measures are implemented as per the TCP, and check that public traffic, transit, and construction traffic is managed in accordance with the S-TTMP. He/she will mark up any variations or omissions in site detail that may affect the traffic and transit operations for the duration of the traffic setup on a copy of the S-TTMP as a Daily Traffic Management Site Record. These records will be dated and signed by the Traffic Control Supervisor. Copies of these records will be forwarded to the Lead Traffic and Mobility Representative within 48 hours of their preparation.

### Daily Traffic Monitoring Reports

The Daily Traffic Monitoring Report is used to document traffic conditions for all modes of transportation observed for the duration of the traffic control setup. A sample of the Daily Traffic Monitoring Report is shown in the **Figure 9** below.





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Revision: 02	Date: 2020/10/01	Owner: Traffic Manager


<b>Site Name:</b>		<b>Date:</b>		<b>Project Week No:</b>	
<b>Active Construction Zone/Activity:</b>		<b>Time to Visit (from/to)</b>		<b>Segment No.</b>	
<b>Observer (Traffic Control Supervisor) and Contact Tel No.</b>		<b>Document Control No.</b>			
<b>Item</b>	<b>Description</b>	<b>YES, NO, or N/A</b>	<b>Notes (Issues Identified)/ Comments</b>	<b>Action required by TransitNEXT (Y/N)</b>	<b>Action required by the CITY (Y/N)</b>
1	Duration of temporary Lane (Traffic/BRT, bike) closures in place?				
2	Were any full lane closures (long term/ permanent nature – caused by Trillium related construction activities) in place?				
3	Sidewalk, pathway, MUP TEMPORARY closures in place?				
4	Sidewalk, pathway, MUP LONG closures in place?				
5	Is Sidewalk, MUP, or Cycling Lane surface condition good?				
6	Detours and Lane Shifts?				
7	Traffic Delays/Length of Traffic Queues? (Likely cause)				
8	Pedestrian Behaviour at crossings, Unauthorized crossing, Issues if any?				
9	Loading and Unloading Activities? (unauthorized?)				
10	Parking and special use zone issues?				
11	Cyclist Behaviour, issues if any?				
12	Difficulty for pedestrian and vehicular access to adjacent properties and businesses?				
13	Flag Person or Sub-contractor's personnel available on site, Security presence ... etc.?				

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14	Observations from City's Traffic Cams? (issues noted)				
15	Transit Travel Times? (issues if any)				
16	Transit Operational Performance? (issues if any)				
17	TIMG (Traffic Incident Management Group) Activated?				
18	Police Point Duty?				
19	TCPs and ETPs are developed as per OTM Surface good for Walking & Cycling? they are kept up to date with necessary modifications during the Project?				
20	All traffic signs and features are in place?				
21	Incidents? (such as accidents ... etc.)				
22	Notes gathered from field Engineer or Sub-contractor's flag person (traffic related issues only)?				
23	Comments from OC Transpo's Supervisor (if applicable)?				
24	Any risks identified in the TTMP observed?				
25	Other comments				

Signed off: \_\_\_\_\_  
Traffic Supervisor

**Figure 9. Sample Daily Traffic Monitoring Report**

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Copies of these reports will be forwarded to the Lead Traffic and Mobility Representative each day whenever traffic control setups are in place. A monthly summary of these reports will be prepared and brought to the Traffic Management Committee Meetings for review and discussion.

### Daily Lane Closure Reports

Whenever lane closures are implemented to accommodate construction activities, Daily Lane Closure Reports will be created to support the monthly Lane Closure Analysis Report in accordance with Schedule 7 – Mobility Matters. As of publication, Daily Lane Closure reports are completed two times per week, and are submitted monthly to the City and the Lead Traffic and Mobility Representative.

## 5.10 LANE CLOSURE MEASUREMENT AND VERIFICATION PLAN

TransitNEXT will plan lane closures to meet the requirements of Schedule 7 in our Lane Closure Target summary spreadsheet. These will be verified monthly and summarized in the Lane Closure Analysis Report. The Daily Lane Closure Reports from the Traffic Supervisor will serve as records for lane closures. TransitNEXT will evaluate the lane closure progress in relation to the monthly targeted lane closures with our daily records, which are submitted monthly. Copies of these records will be forwarded to the City to determine the Monthly Lane Closure Adjustment Contribution.

## 5.11 PROPERTY ACCESS AND BUSINESS CONTINUITY PLAN

Residential and business access will be maintained at all times. If pedestrian or vehicular access is affected by the construction traffic control measures, TransitNEXT Community Liaison staff will talk with affected business owners and residents to solicit consent to any of the mitigation options below:


- Temporary relocation of access
- Construction of the temporary access if required
- Limitation of inaccessibility to late nighttime hours for the shortest time possible

To minimize impact on businesses adjacent to construction sites, we will delimit our work zones away from the business entrances as much as possible. In addition, business signage will be maintained; however, if any construction equipment or fences create visual obstruction to these signs, temporary logo signage with directional arrows will be installed to maintain business visibility.

Refer to the communications plan for Community Liaison process.

## 6 Partial S-TTMP and Full S-TTMP

It is important to note that not every sub-plan is required to support every proposed TCP. In some cases a simple Traffic Control Plan (TCP) or signage plan will be sufficient as no other impacts need to be addressed through other sub-plans. Cases such as **short duration** MUP closures, sidewalk closures, and shoulder closures create no impact on vehicular traffic and therefore may not require any supportive sub-plans. In any case it may be unclear, TransitNEXT Traffic team will work with the City Traffic Representatives to determine if a Partial S-TTMP or full S-TTMP will be required by any proposed traffic control/closures.

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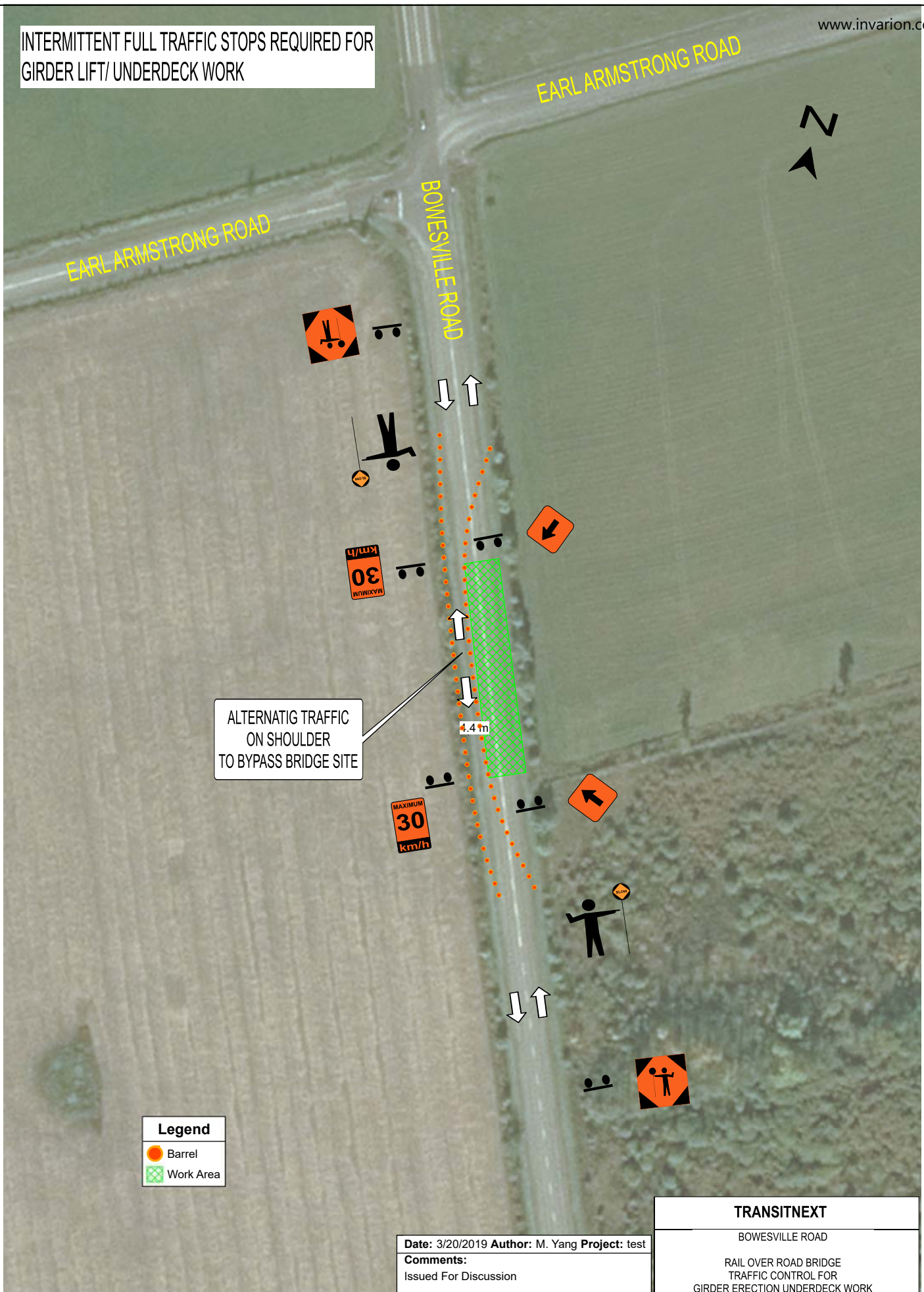
## APPENDIX A

# TRAFFIC CONTROL CONCEPT PLANS

# APPENDIX A – TRAFFIC CONTROL CONCEPT PLANS

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EARL ARMSTRONG RAIL OVER BRIDGE – GIRDER ERECTION & UNDERDECK WORK .....	5
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LIMEBANK RAIL OVER ROAD BRIDGE – MEDIAN UNDERDECK WORK.....	27

INTERMITTENT FULL TRAFFIC STOPS REQUIRED FOR GIRDER LIFT/ UNDERDECK WORK



ALTERNATIG TRAFFIC ON SHOULDER TO BYPASS BRIDGE SITE

4.4 m

Legend	
	Barrel
	Work Area

**Date:** 3/20/2019 **Author:** M. Yang **Project:** test  
**Comments:**  
 Issued For Discussion

<b>TRANSITNEXT</b>
BOWESVILLE ROAD
RAIL OVER ROAD BRIDGE TRAFFIC CONTROL FOR GIRDER ERECTION UNDERDECK WORK





# FLAGGING AND FULL CLOSURE GIRDER ERECTION



MAINTAIN 1 LANE/DIRECTION UNTIL INTERMITTENT FULL CLOSURE WITH FLAGGING

**Legend**

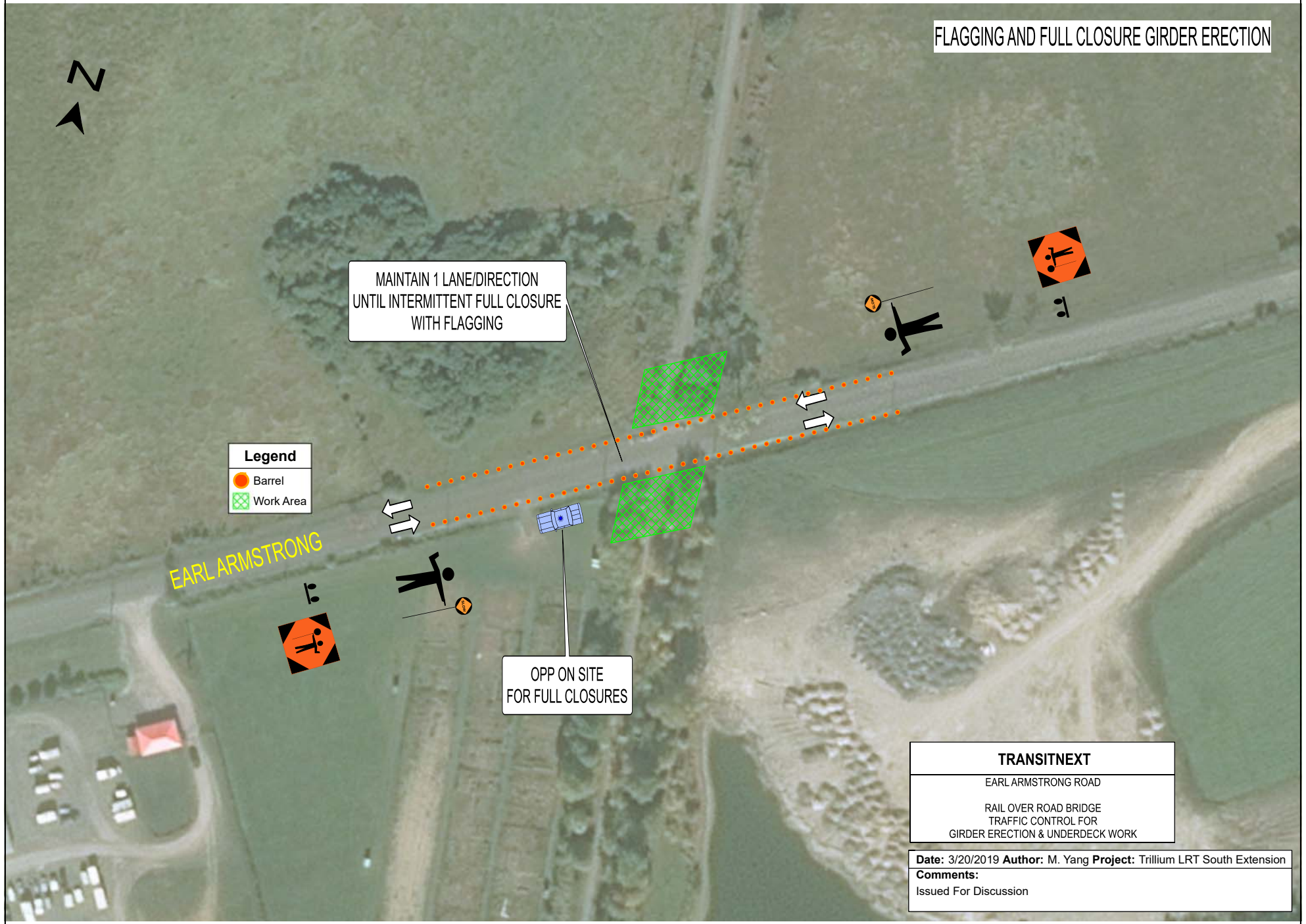
- Barrel
- Work Area

EARL ARMSTRONG

OPP ON SITE FOR FULL CLOSURES

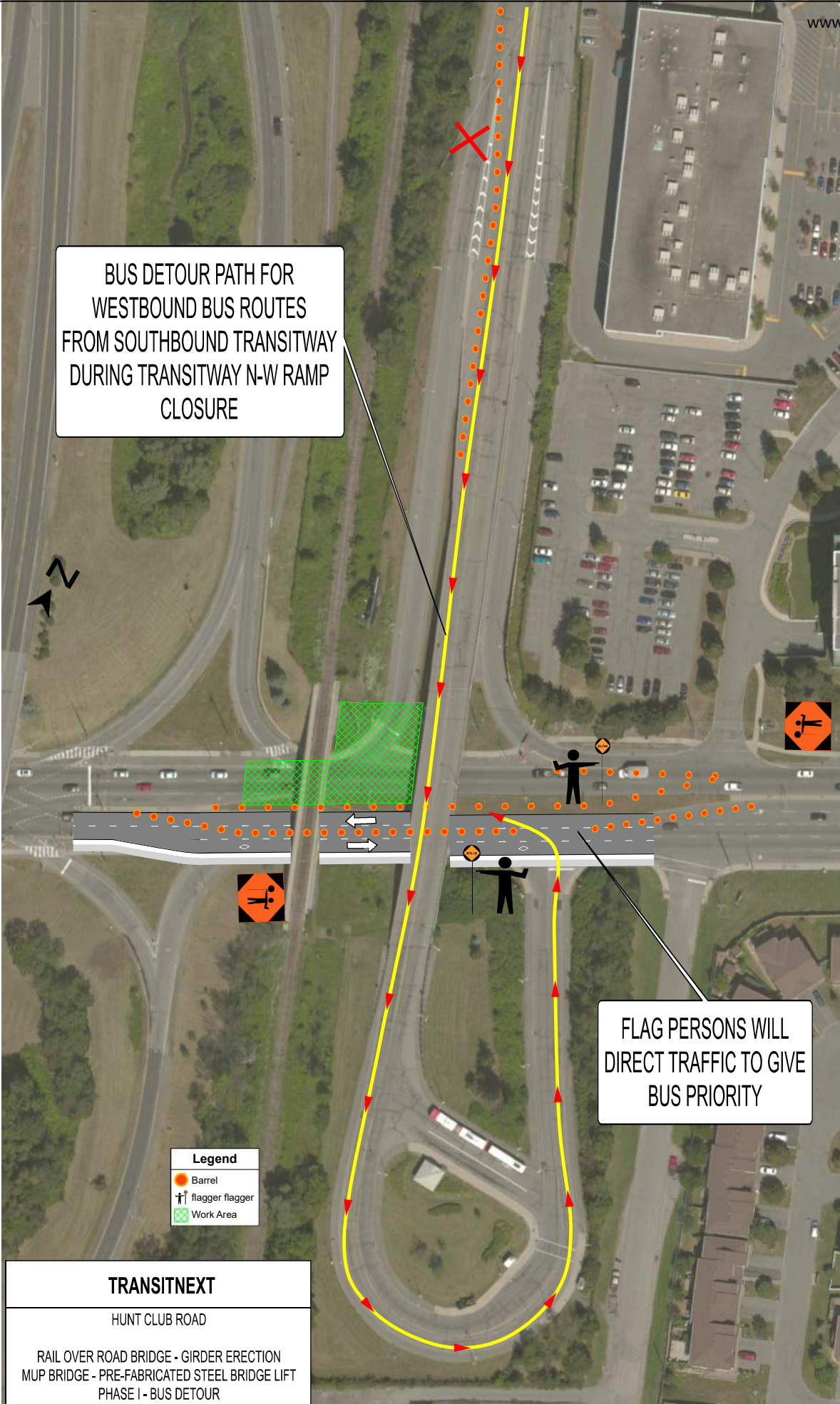
<b>TRANSITNEXT</b>
EARL ARMSTRONG ROAD
RAIL OVER ROAD BRIDGE TRAFFIC CONTROL FOR GIRDER ERECTION & UNDERDECK WORK

**Date:** 3/20/2019 **Author:** M. Yang **Project:** Trillium LRT South Extension  
**Comments:**  
Issued For Discussion





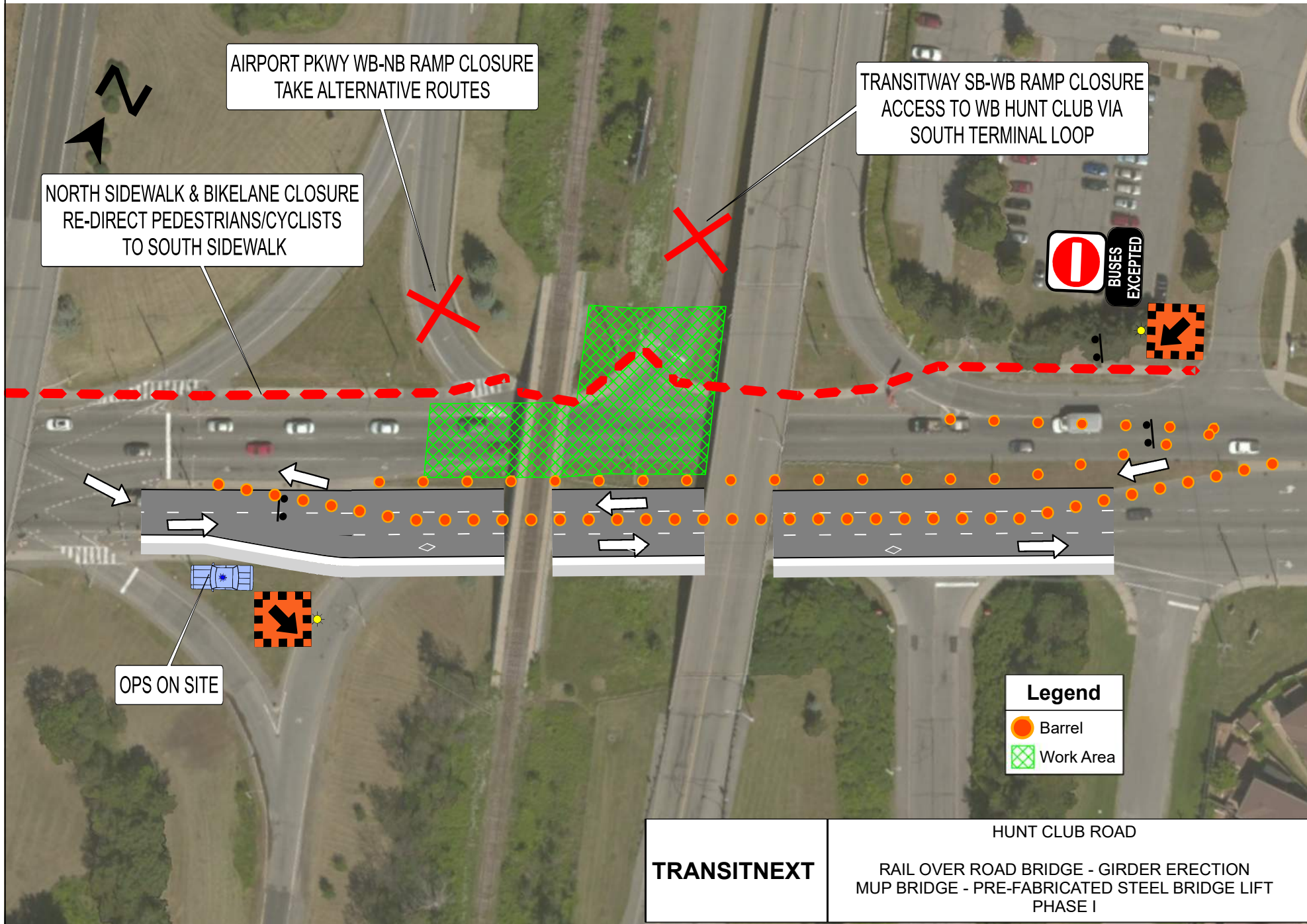
BUS DETOUR PATH FOR WESTBOUND BUS ROUTES FROM SOUTHBOUND TRANSITWAY DURING TRANSITWAY N-W RAMP CLOSURE



FLAG PERSONS WILL DIRECT TRAFFIC TO GIVE BUS PRIORITY

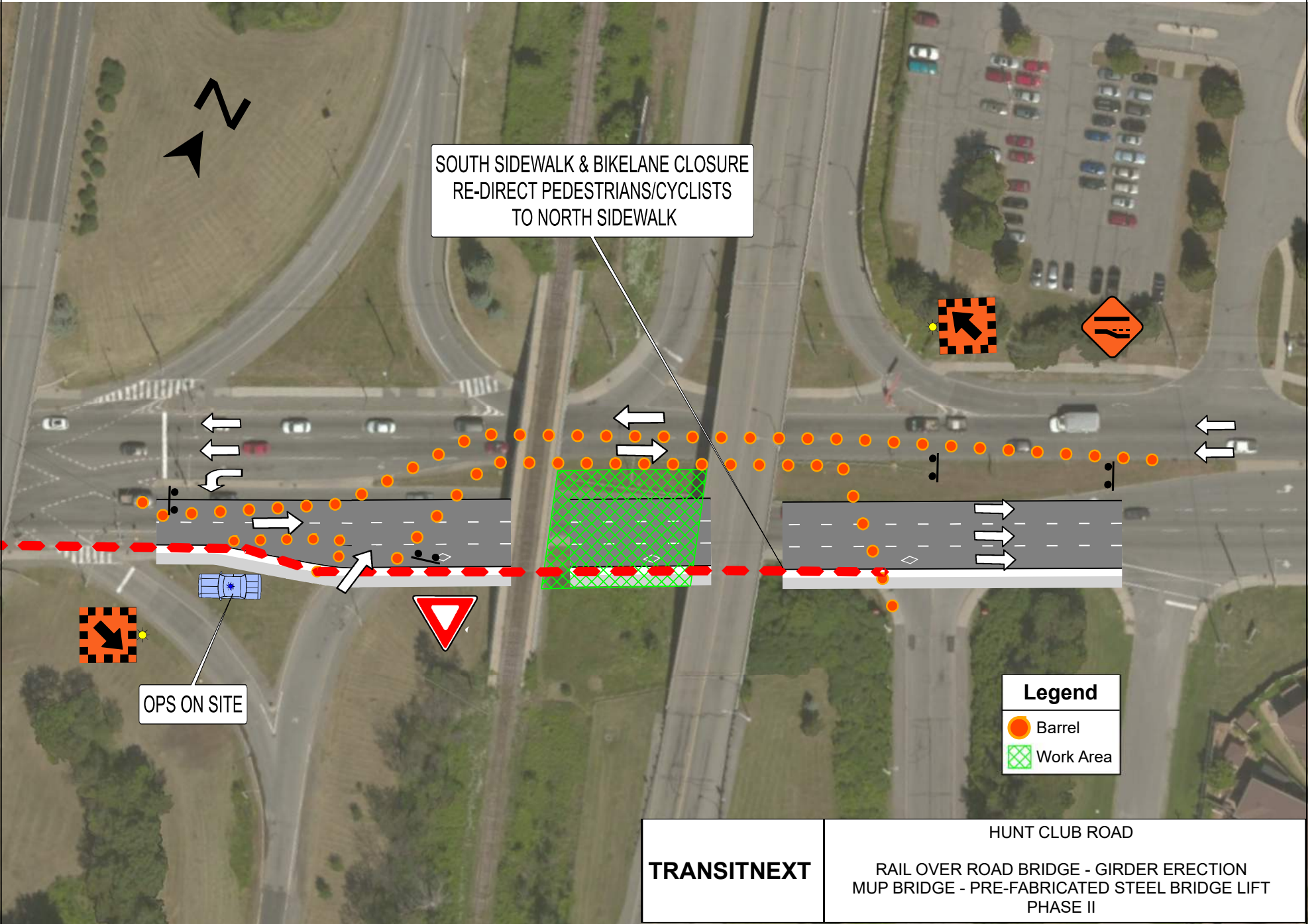
- Legend**
- Barrel
  - ⚠ flagger flagger
  - ▨ Work Area

**TRANSITNEXT**  
HUNT CLUB ROAD  
RAIL OVER ROAD BRIDGE - GIRDER ERECTION  
MUP BRIDGE - PRE-FABRICATED STEEL BRIDGE LIFT  
PHASE I - BUS DETOUR



<b>TRANSITNEXT</b>	<b>HUNT CLUB ROAD</b>
	RAIL OVER ROAD BRIDGE - GIRDER ERECTION MUP BRIDGE - PRE-FABRICATED STEEL BRIDGE LIFT PHASE I





SOUTH SIDEWALK & BIKELANE CLOSURE  
RE-DIRECT PEDESTRIANS/CYCLISTS  
TO NORTH SIDEWALK

OPS ON SITE

**Legend**

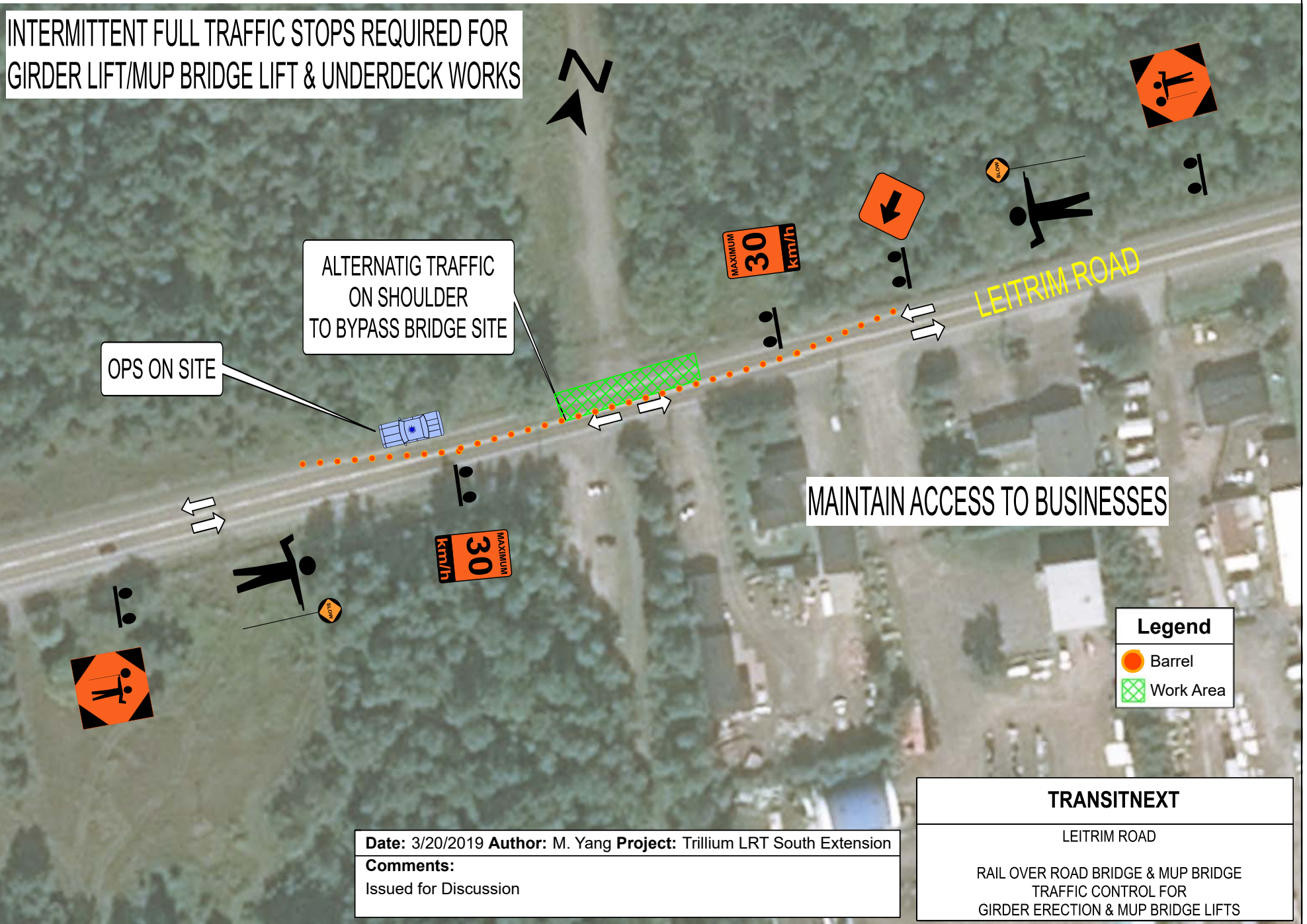
- Barrel
- Work Area

**TRANSITNEXT**

HUNT CLUB ROAD

RAIL OVER ROAD BRIDGE - GIRDER ERECTION  
MUP BRIDGE - PRE-FABRICATED STEEL BRIDGE LIFT  
PHASE II

# INTERMITTENT FULL TRAFFIC STOPS REQUIRED FOR GIRDER LIFT/MUP BRIDGE LIFT & UNDERDECK WORKS



ALTERNATIVE TRAFFIC ON SHOULDER TO BYPASS BRIDGE SITE

OPS ON SITE

MAINTAIN ACCESS TO BUSINESSES

**Legend**

- Barrel
- Work Area

**TRANSITNEXT**

LEITRIM ROAD

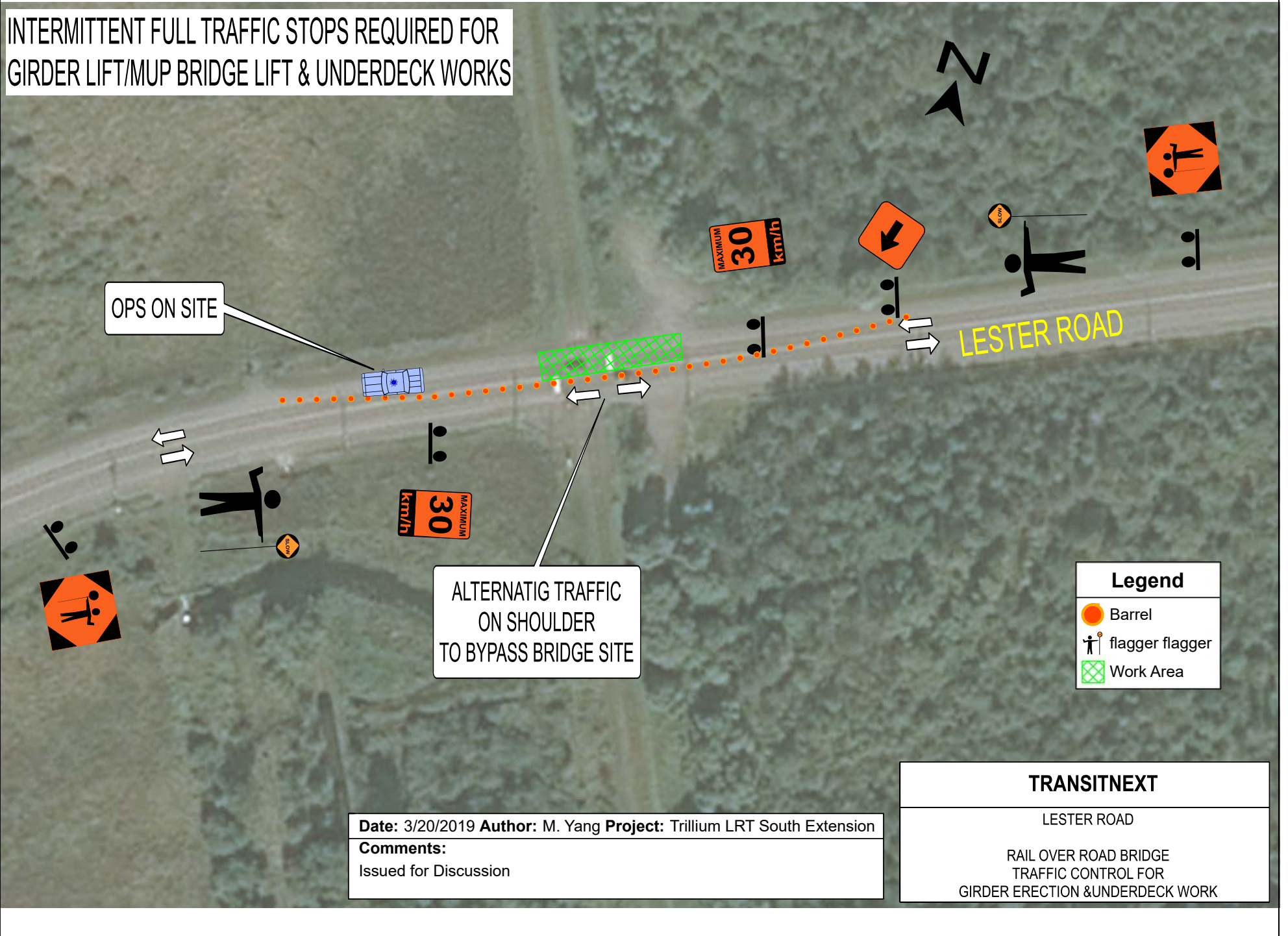
RAIL OVER ROAD BRIDGE & MUP BRIDGE TRAFFIC CONTROL FOR GIRDER ERECTION & MUP BRIDGE LIFTS

**Date:** 3/20/2019 **Author:** M. Yang **Project:** Trillium LRT South Extension

**Comments:**  
Issued for Discussion



# INTERMITTENT FULL TRAFFIC STOPS REQUIRED FOR GIRDER LIFT/MUP BRIDGE LIFT & UNDERDECK WORKS



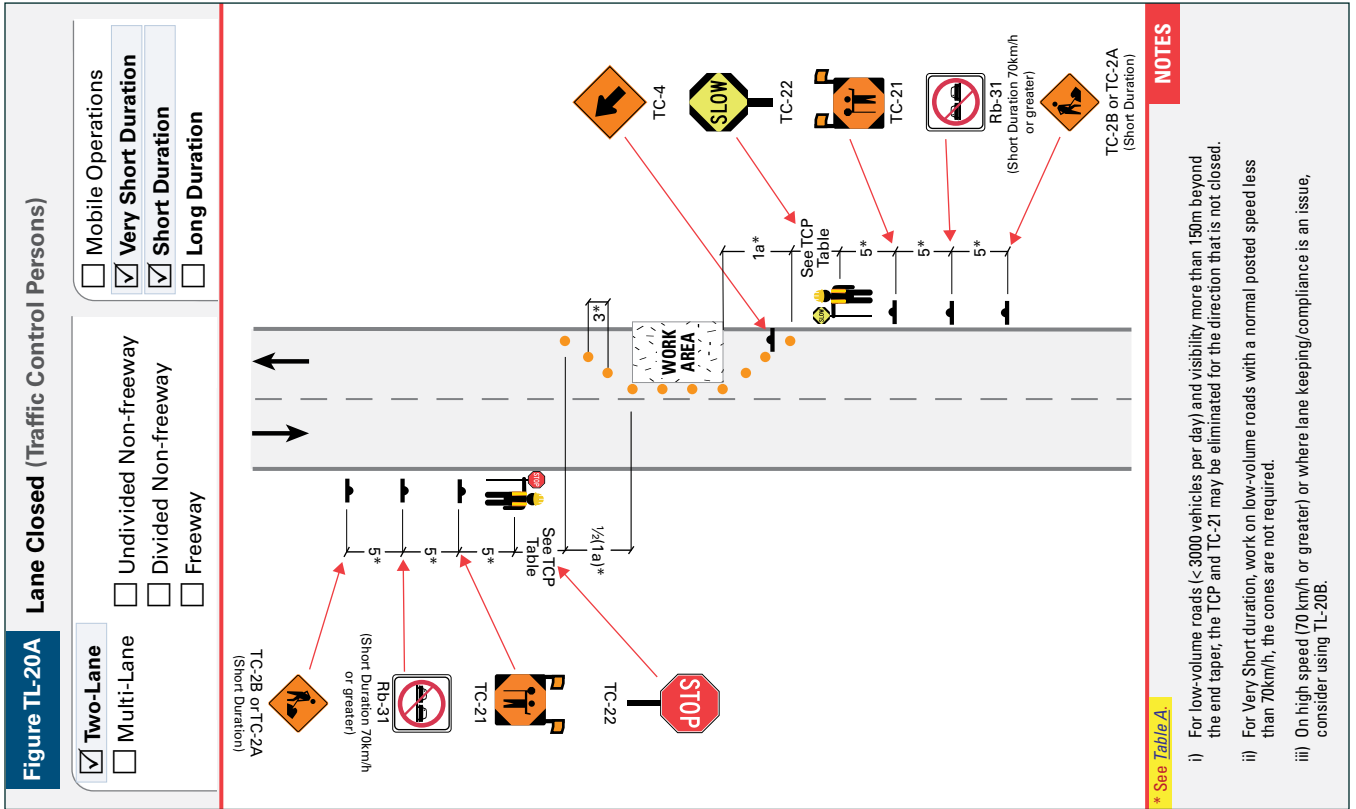
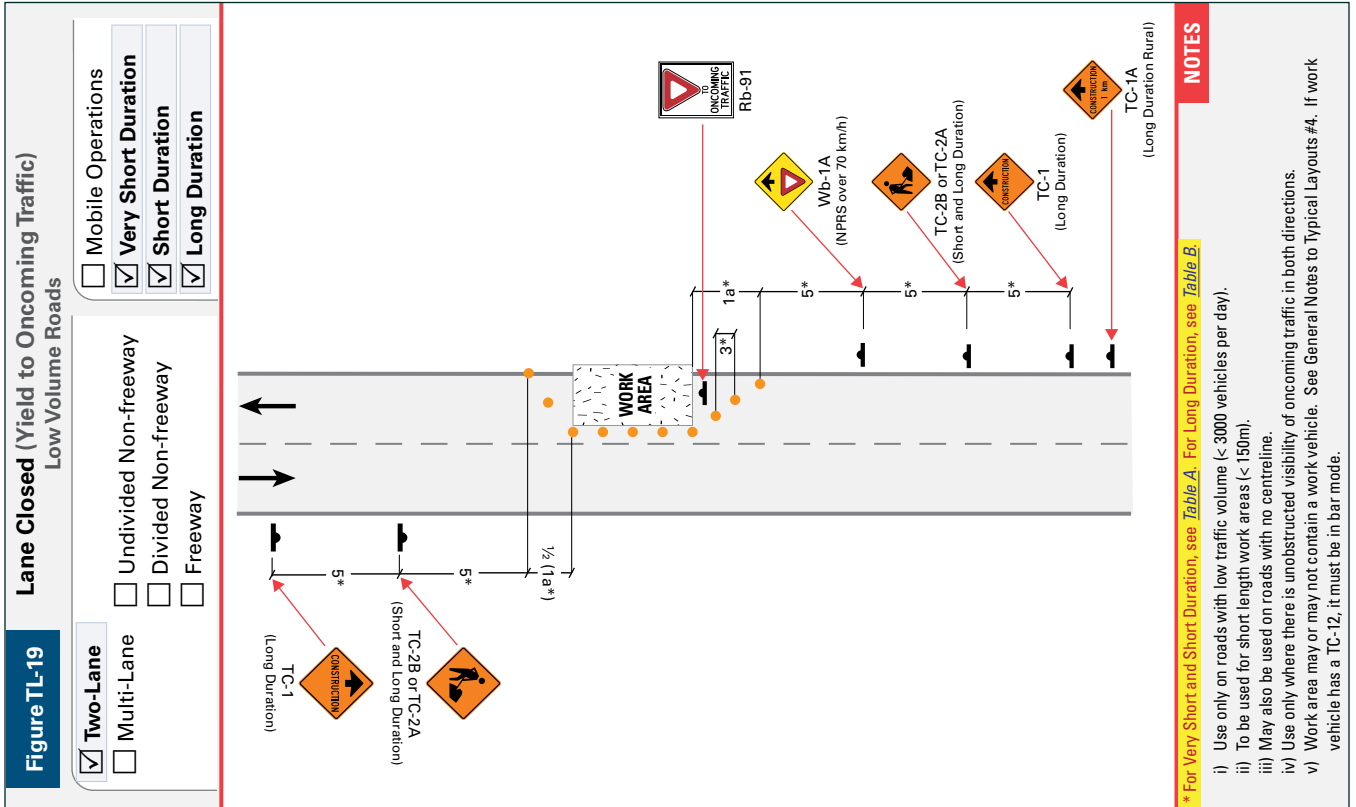
OPS ON SITE

ALTERNATIG TRAFFIC ON SHOULDER TO BYPASS BRIDGE SITE

Legend	
	Barrel
	flagger flagger
	Work Area

**Date:** 3/20/2019 **Author:** M. Yang **Project:** Trillium LRT South Extension  
**Comments:**  
Issued for Discussion

**TRANSITNEXT**  
LESTER ROAD  
RAIL OVER ROAD BRIDGE  
TRAFFIC CONTROL FOR  
GIRDER ERECTION & UNDERDECK WORK



## **SEGMENT 2**

- CARLETON STATION – MULTI-USE PATH DETOUR DURING MUP UNDERPASS RECONSTRUCTION
- ELLWOOD DIAMOND – FULL CLOSURE FOR GIRDER ERECTION & UNDERDECK WORK
- ELLWOOD DIAMOND – TRAFFIC CONTROL FOR PIER #3, CAISSON PILE & PILE WORK
- ELLWOOD DIAMOND – TRAFFIC CONTROL FOR PIER #4, CAISSON PILE & PILE WORK
- SOUTH KEYS STATION – TRAFFIC CONTROL FOR SITE ACCESS/EGRESS



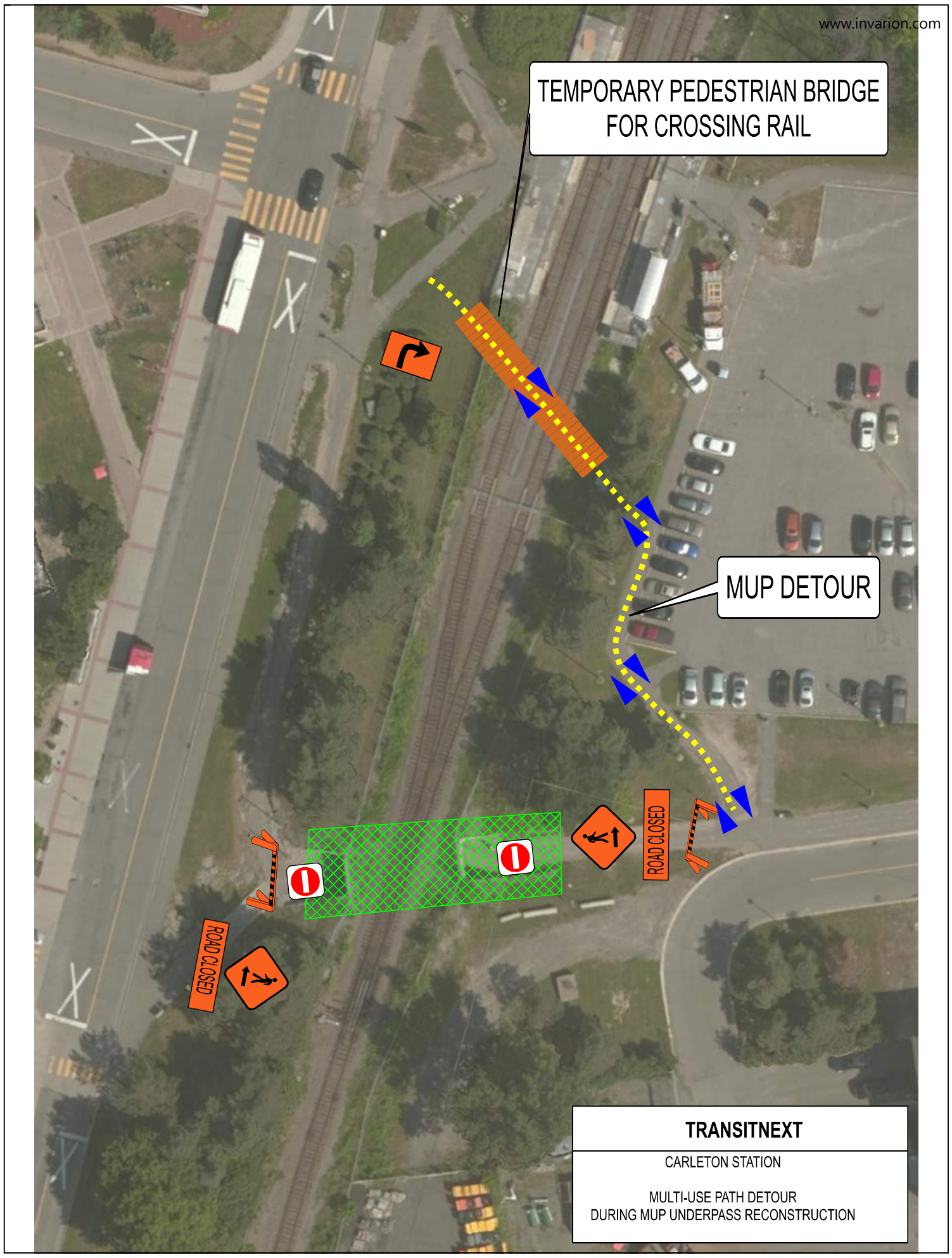
TEMPORARY PEDESTRIAN BRIDGE FOR CROSSING RAIL

MUP DETOUR

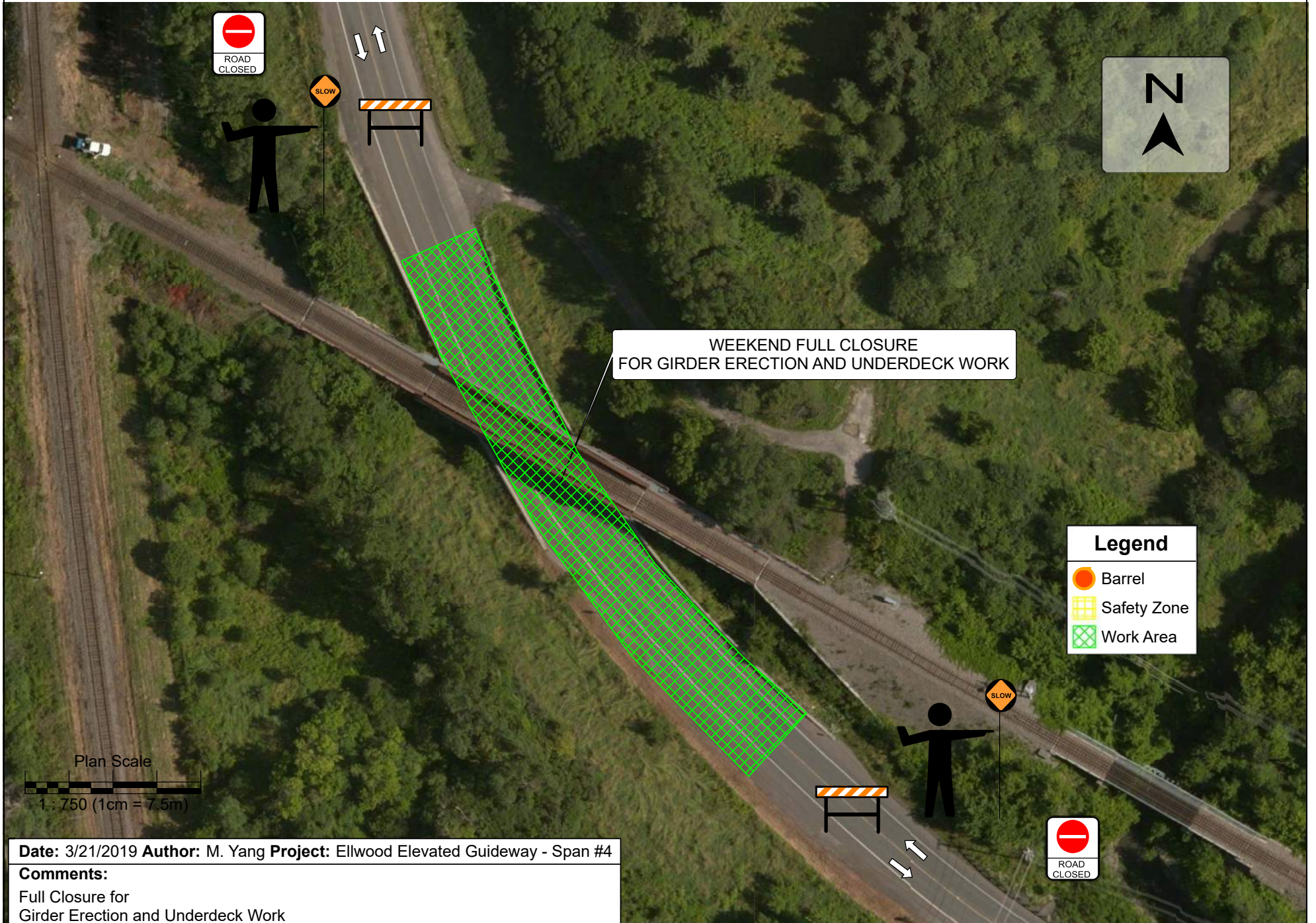
**TRANSITNEXT**

CARLETON STATION

MULTI-USE PATH DETOUR  
DURING MUP UNDERPASS RECONSTRUCTION



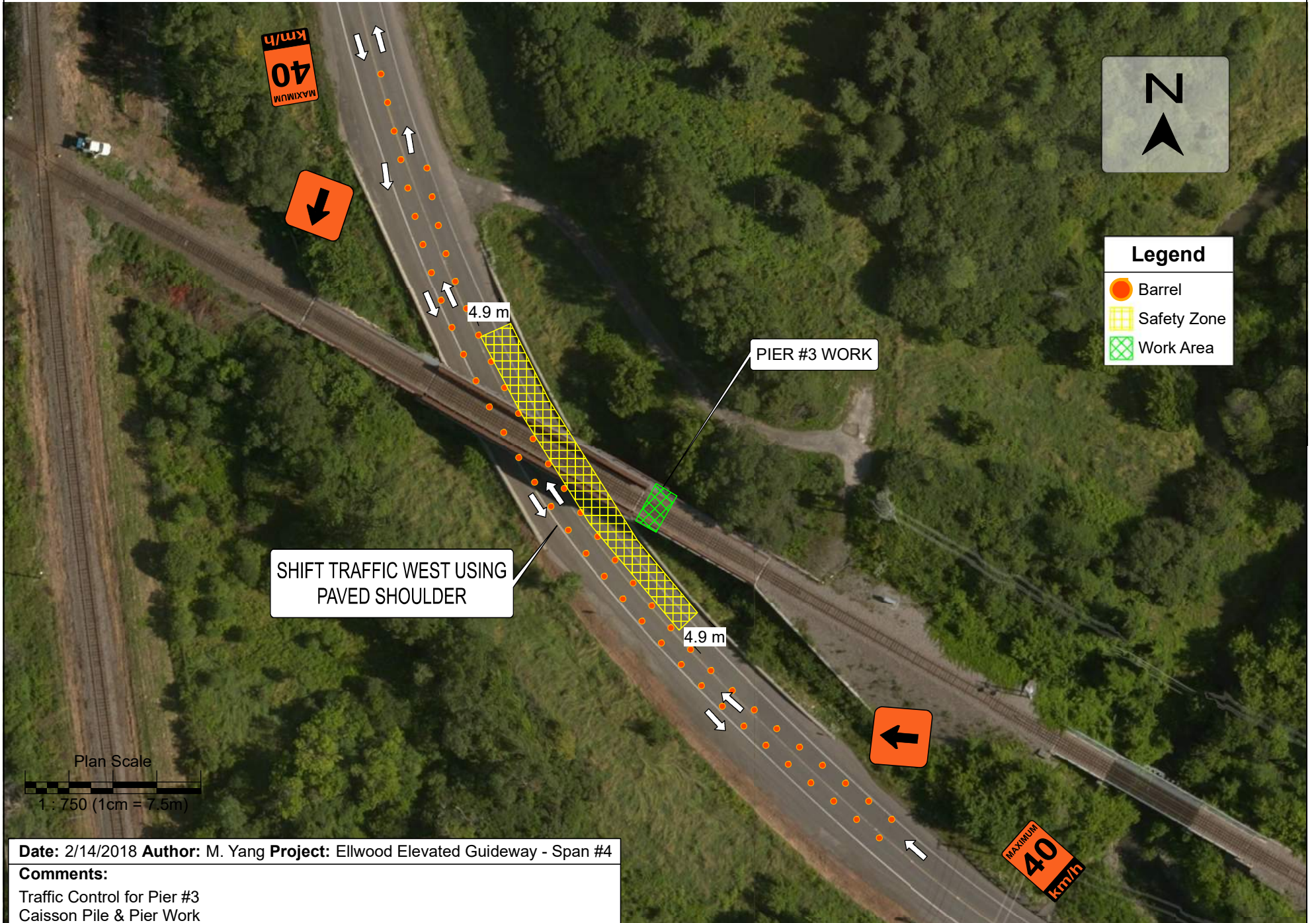




**Date:** 3/21/2019 **Author:** M. Yang **Project:** Ellwood Elevated Guideway - Span #4

**Comments:**  
Full Closure for  
Girder Erection and Underdeck Work



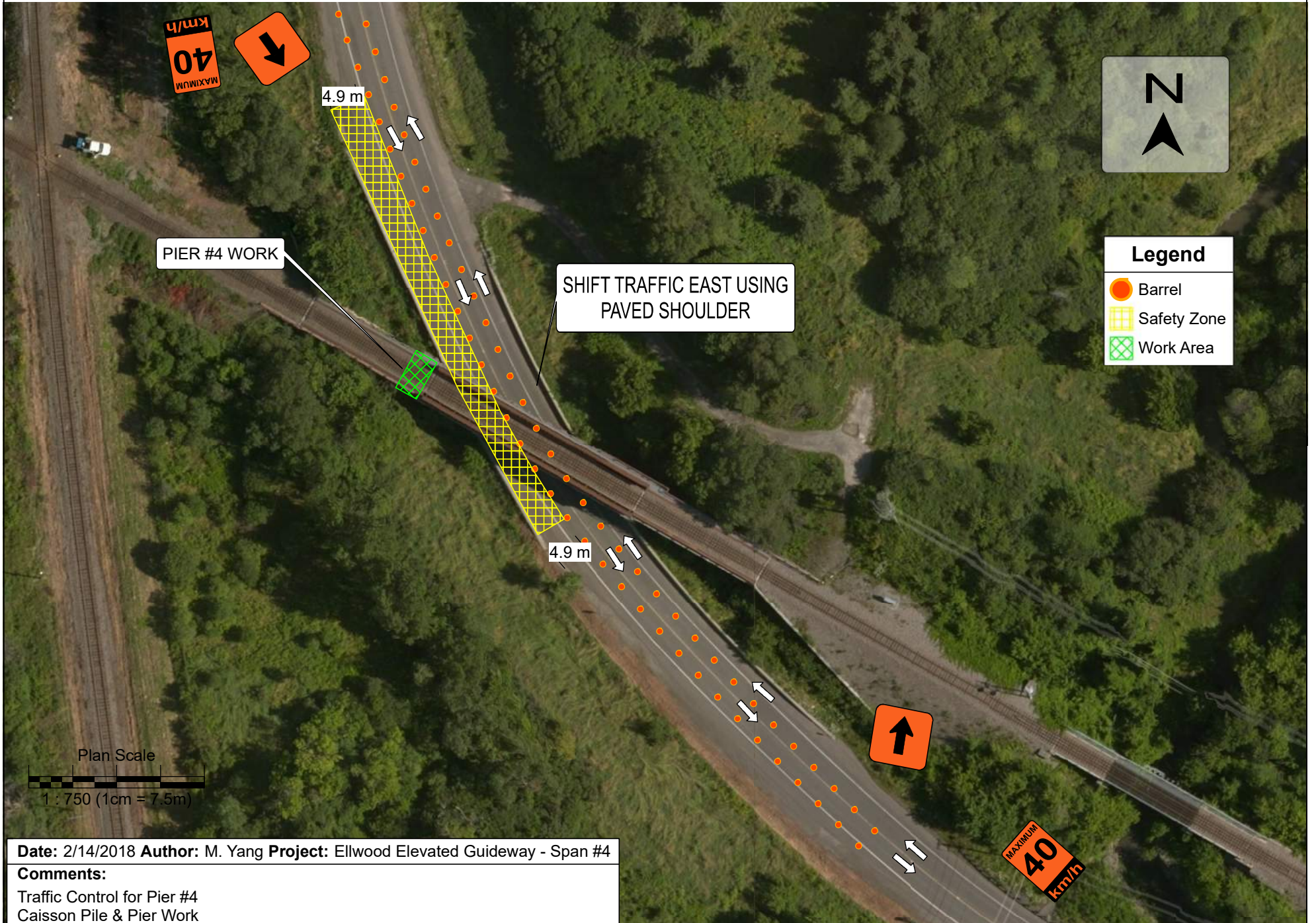


**Date:** 2/14/2018 **Author:** M. Yang **Project:** Ellwood Elevated Guideway - Span #4

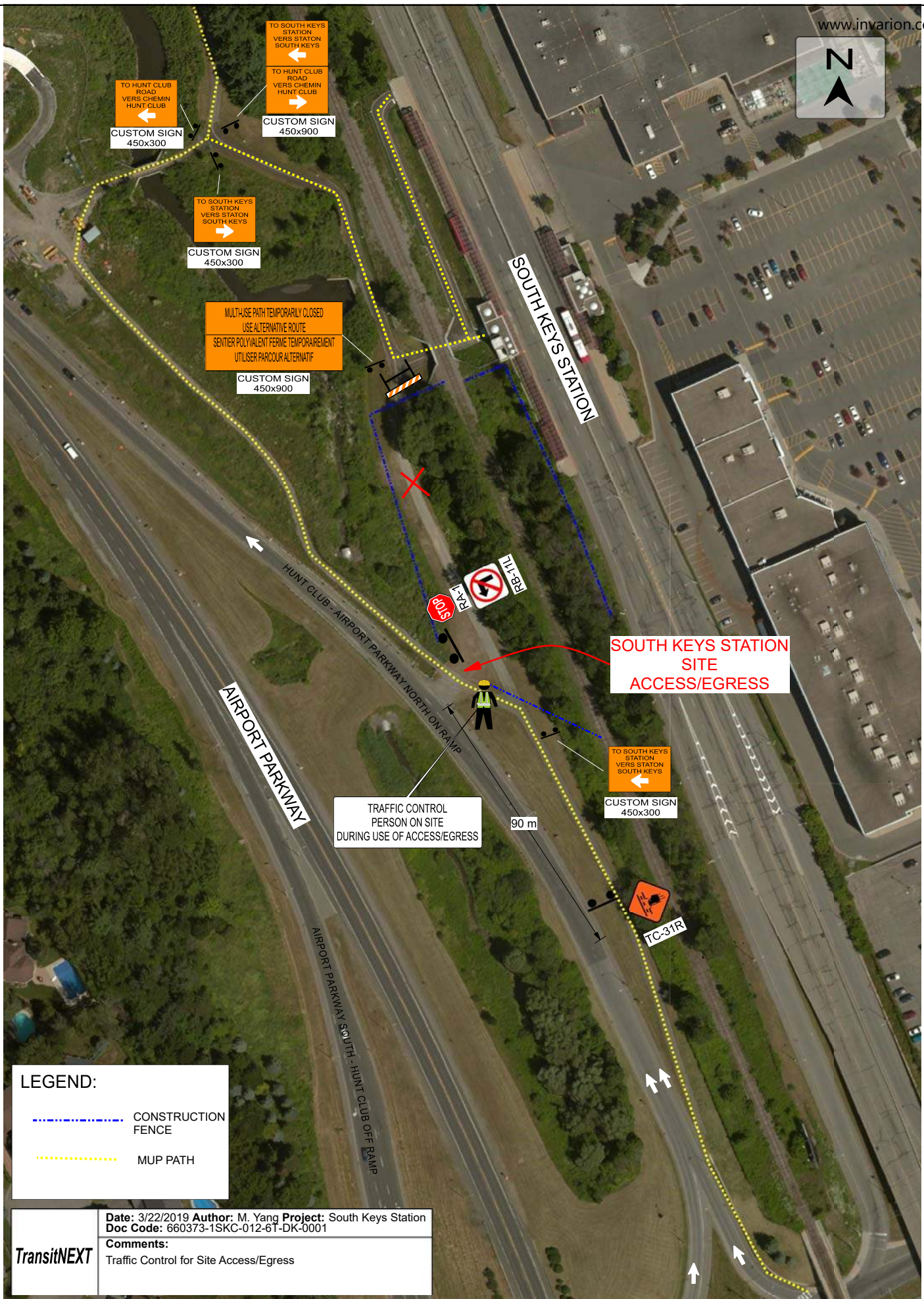
**Comments:**

Traffic Control for Pier #3  
Caisson Pile & Pier Work









TO HUNT CLUB ROAD  
VERS CHEMIN HUNT CLUB  
←  
CUSTOM SIGN  
450x300

TO SOUTH KEYS STATION  
VERS STATION SOUTH KEYS  
←  
CUSTOM SIGN  
450x900

TO HUNT CLUB ROAD  
VERS CHEMIN HUNT CLUB  
→  
CUSTOM SIGN  
450x900

TO SOUTH KEYS STATION  
VERS STATION SOUTH KEYS  
→  
CUSTOM SIGN  
450x300

MULTI-USE PATH TEMPORARILY CLOSED  
USE ALTERNATIVE ROUTE  
SENTIER POLYVALENT FERME TEMPORAIREMENT  
UTILISER PARCOURS ALTERNATIF  
CUSTOM SIGN  
450x900

TRAFFIC CONTROL PERSON ON SITE DURING USE OF ACCESS/EGRESS

SOUTH KEYS STATION SITE ACCESS/EGRESS

TO SOUTH KEYS STATION  
VERS STATION SOUTH KEYS  
←  
CUSTOM SIGN  
450x300

**LEGEND:**

--- CONSTRUCTION FENCE

--- MUP PATH

Date: 3/22/2019 Author: M. Yang Project: South Keys Station  
Doc Code: 660373-1SKC-012-6T-DK-0001

Comments:  
Traffic Control for Site Access/Egress

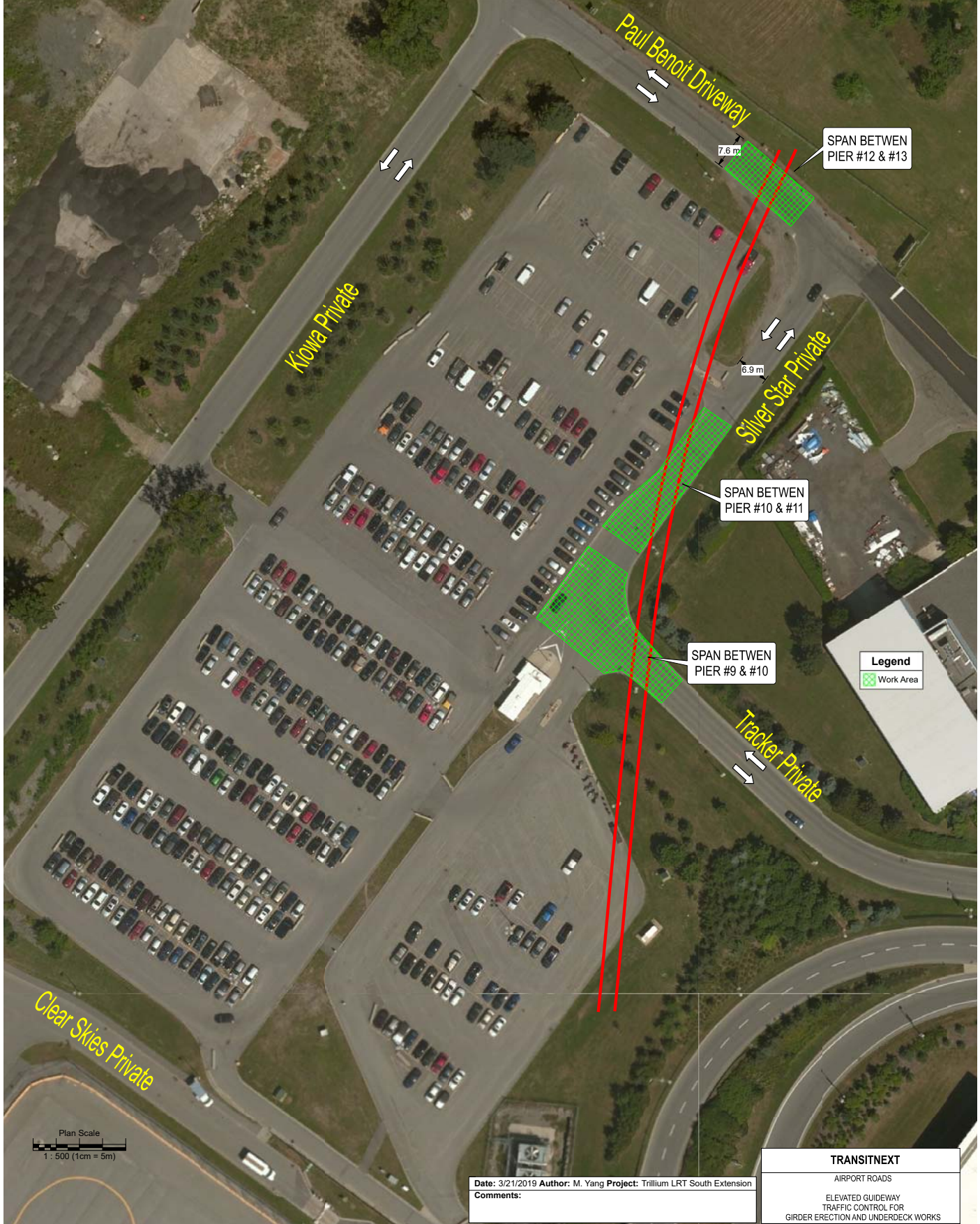
TransitNEXT

## **SEGMENT 3**

- AIRPORT ELEVATED GUIDEWAY – GIRDER ERECTION & UNDERDECK WORKS
- AIRPORT PARKWAY RAIL OVER ROAD BRIDGE – GIRDER ERECTION & UNDERDECK WORKS
- UPLANDS RAIL OVER ROAD BRIDGE – GIRDER ERECTION & UNDERDECK WORKS



3 SITES REQUIRING TRAFFIC CONTROL WITH FLAGGERS  
7 NIGHTS SINGLE LANE CLOSURE FOR EACH SITE  
FOLLOW OTM BOOK 7 TL-20A



**Legend**  
Work Area

Plan Scale  
1 : 500 (1cm = 5m)

Date: 3/21/2019 Author: M. Yang Project: Trillium LRT South Extension  
Comments:

**TRANSITNEXT**  
AIRPORT ROADS  
ELEVATED GUIDEWAY  
TRAFFIC CONTROL FOR  
GIRDER ERECTION AND UNDERDECK WORKS



INTERMITTENT FULL TRAFFIC STOPS REQUIRED FOR GIRDER LIFT & UNDERDECK WORKS  
REDUCE POSTED SPEED FROM 80KM/H TO 60KM/H



**Legend**

- Barrel
- flagger flagger
- Work Area

AIRPORT PARKWAY



ALTERNATIG TRAFFIC ON SHOULDER TO BYPASS BRIDGE SITE

OPS ON SITE



**TRANSITNEXT**

AIRPORT PARKWAY

RAIL OVER ROAD BRIDGE  
TRAFFIC CONTROL FOR  
GIRDER ERECTION & UNDERDECK WORK

Date: 3/20/2019 Author: M. Yang Project: Trillium LRT South Extension  
Comments:  
Issued for Discussion

INTERMITTENT FULL TRAFFIC STOPS REQUIRED FOR GIRDER LIFT & UNDERDECK WORKS

**Legend**

- Barrel
- flagger flagger
- Work Area

ALTERNATIG TRAFFIC ON SHOULDER TO BYPASS BRIDGE SITE

UPLANDS DRIVE

OPS ON SITE

EY CENTRE -AVOID EVENTS

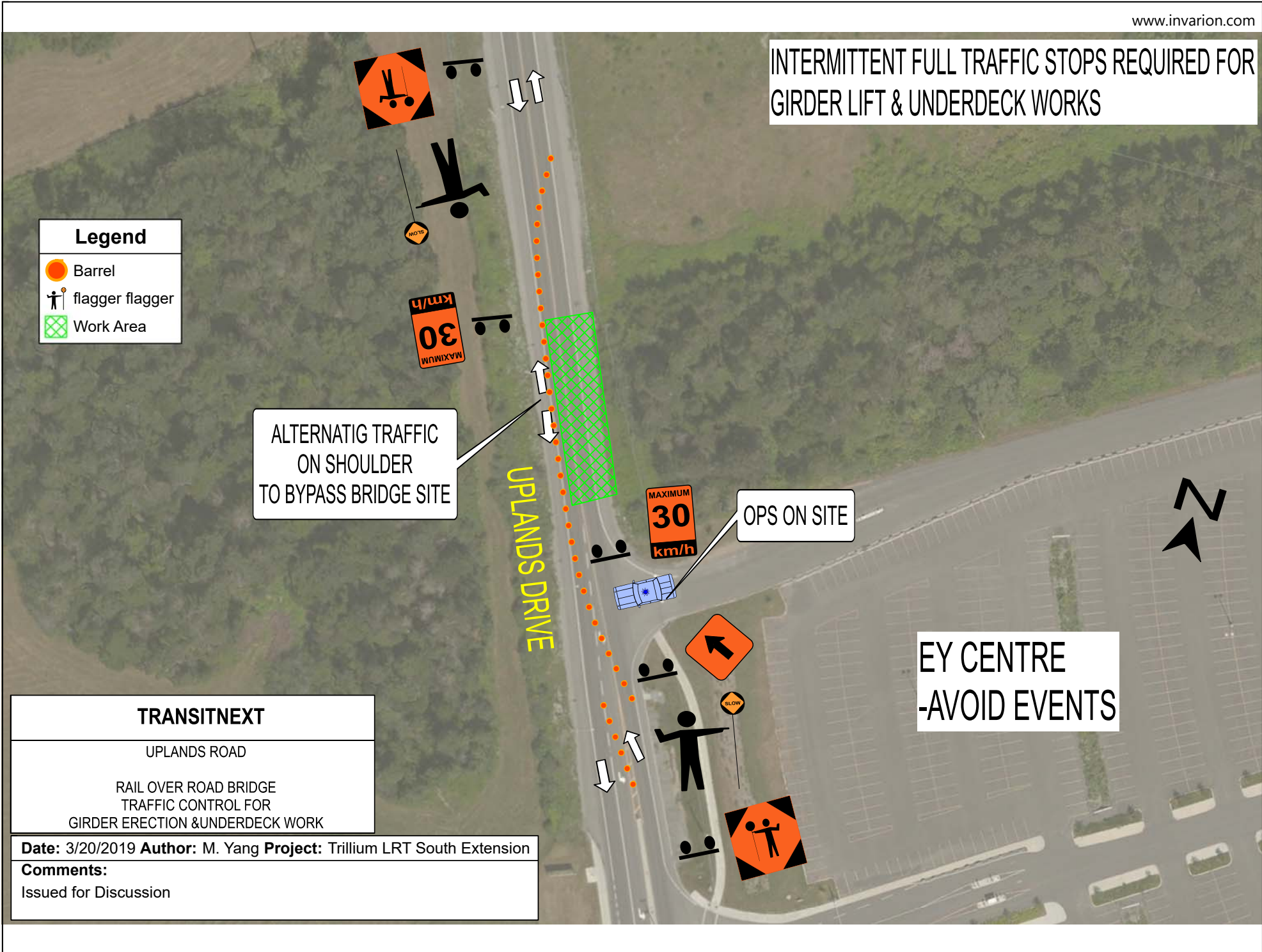
**TRANSITNEXT**

UPLANDS ROAD

RAIL OVER ROAD BRIDGE TRAFFIC CONTROL FOR GIRDER ERECTION & UNDERDECK WORK

**Date:** 3/20/2019 **Author:** M. Yang **Project:** Trillium LRT South Extension

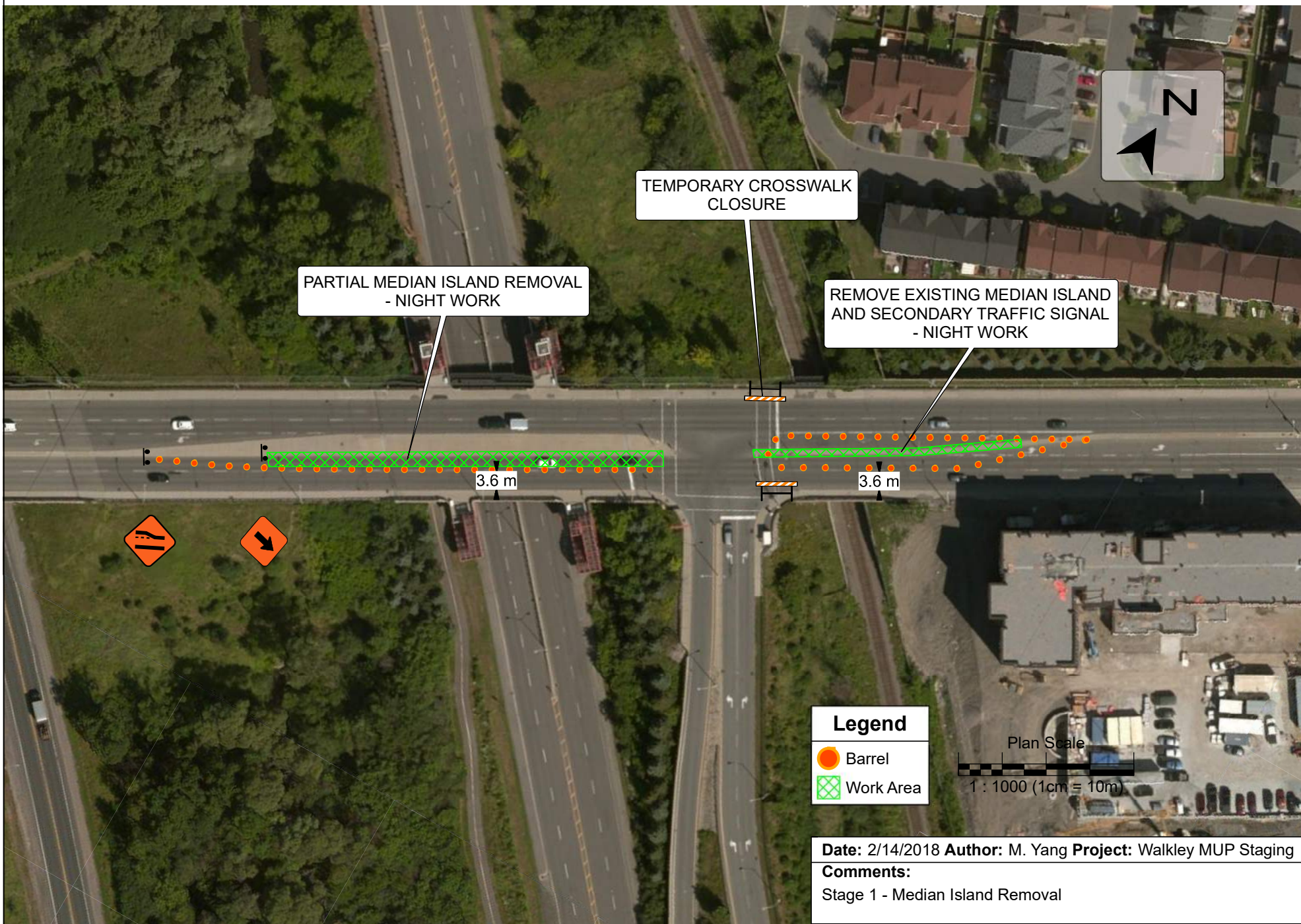
**Comments:**  
Issued for Discussion





## **SEGMENT 4**

- WALKLEY MUP STAGE 1 – MEDIAN ISLAND REMOVAL
- WALKLEY MUP STAGE 2 – MUP CONSTRUCTION



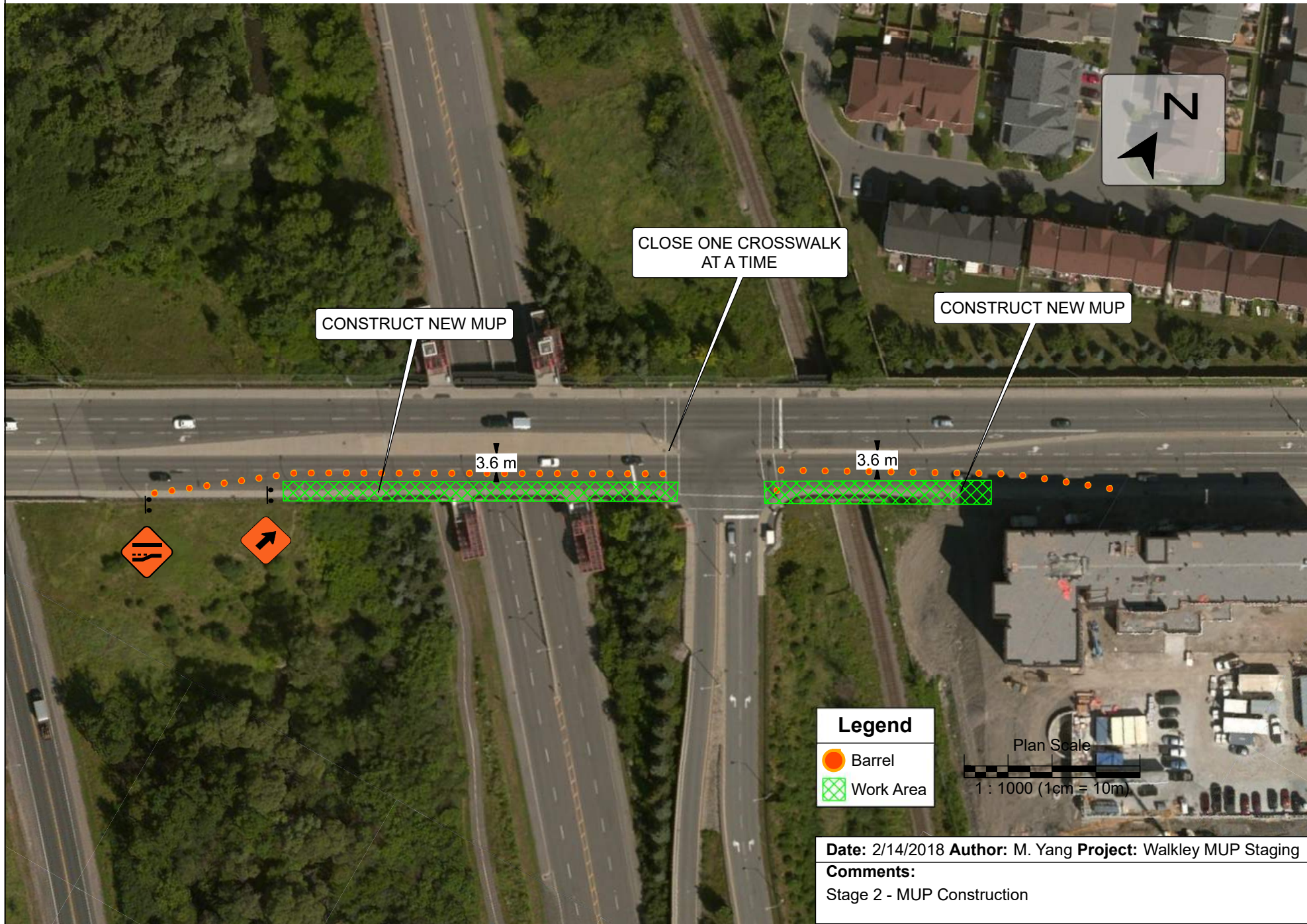
**Legend**

- Barrel
- Work Area

Plan Scale  
1 : 1000 (1cm = 10m)

**Date:** 2/14/2018 **Author:** M. Yang **Project:** Walkley MUP Staging  
**Comments:**  
Stage 1 - Median Island Removal





**Legend**

- Barrel
- Work Area

Plan Scale  
1 : 1000 (1cm = 10m)

**Date:** 2/14/2018 **Author:** M. Yang **Project:** Walkley MUP Staging  
**Comments:**  
Stage 2 - MUP Construction

## **SEGMENT 5**

- LIMEBANK RAIL OVER ROAD BRIDGE – GIRDER ERECTION & UNDERDECK WORK
- LIMEBANK RAIL OVER ROAD BRIDGE – MEDIAN UNDERDECK WORK



LANE CLOSURES OR FLAGGING  
AND FULL CLOSURE GIRDER ERECTION

EARL ARMSTRONG/LIMBANK  
INTERSECTION

OPP ON SITE  
FOR FULL CLOSURES

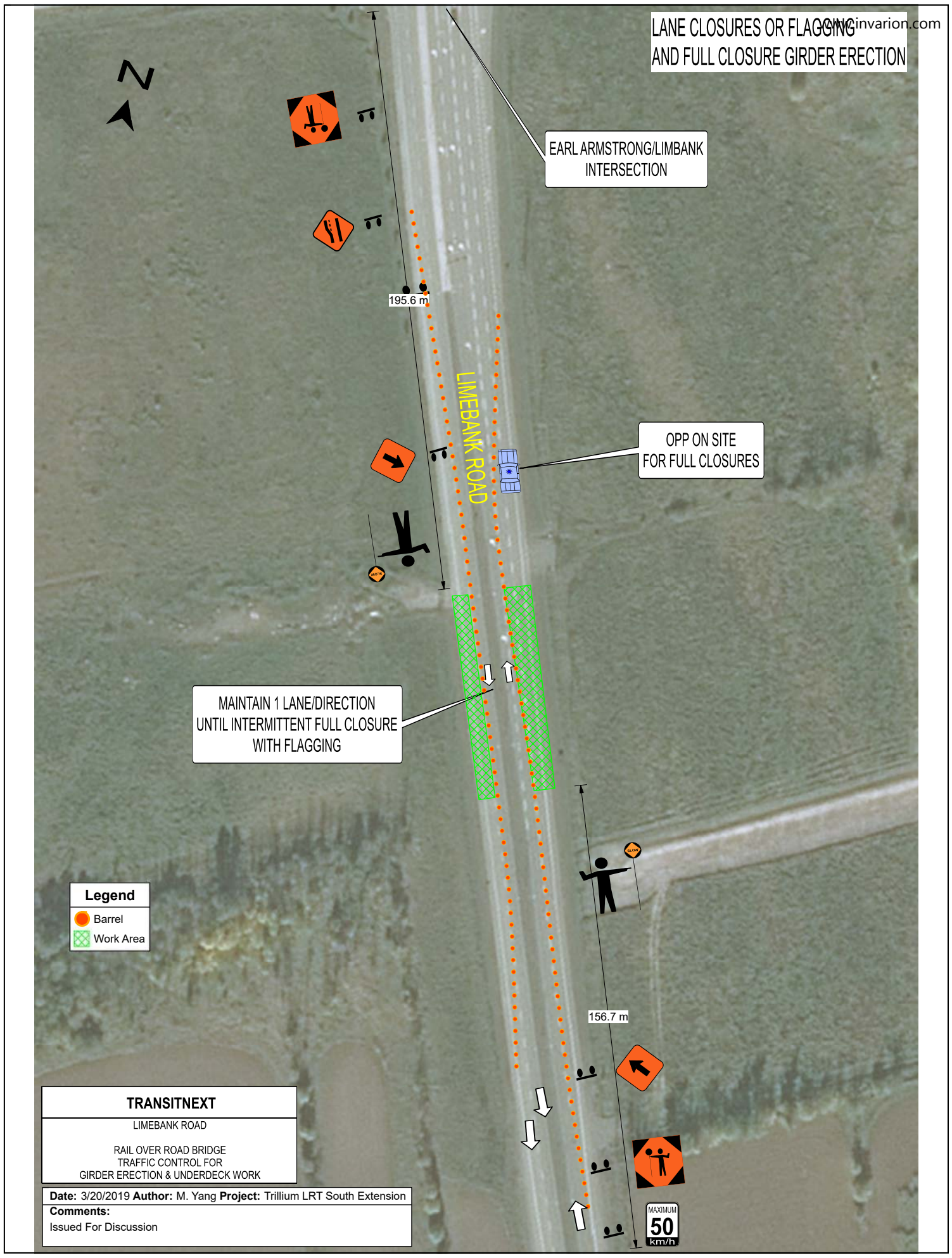
MAINTAIN 1 LANE/DIRECTION  
UNTIL INTERMITTENT FULL CLOSURE  
WITH FLAGGING

**Legend**

- Barrel
- Work Area

**TRANSITNEXT**  
LIMEBANK ROAD  
RAIL OVER ROAD BRIDGE  
TRAFFIC CONTROL FOR  
GIRDER ERECTION & UNDERDECK WORK

**Date:** 3/20/2019 **Author:** M. Yang **Project:** Trillium LRT South Extension  
**Comments:**  
Issued For Discussion







EARL ARMSTRONG/LIMEBANK  
INTERSECTION

196.8 m



LIMEBANK ROAD

MAINTAIN 1 LANE/DIRECTION

MEDIAN UNDERDECK WORK

**Legend**

- Barrel
- Work Area

156.7 m




**TRANSITNEXT**

LIMEBANK ROAD

RAIL OVER ROAD BRIDGE  
TRAFFIC CONTROL FOR  
MEDIAN UNDERDECK WORK

**Date:** 3/20/2019 **Author:** M. Yang **Project:** Trillium LRT South Extension  
**Comments:**  
Issued For Discussion

 <b>SNC • LAVALIN</b>	<b>TRILLIUM LINE EXTENSION PROJECT</b> <b>TRAFFIC AND TRANSIT MANAGEMENT PLAN</b>  660373-0000-003-6TAG-0001	<b>Rev.</b>	<b>Date</b>
		02	2021-03-15

## APPENDIX B

# TRUCK INGRESS/EGRESS ROUTE PLAN

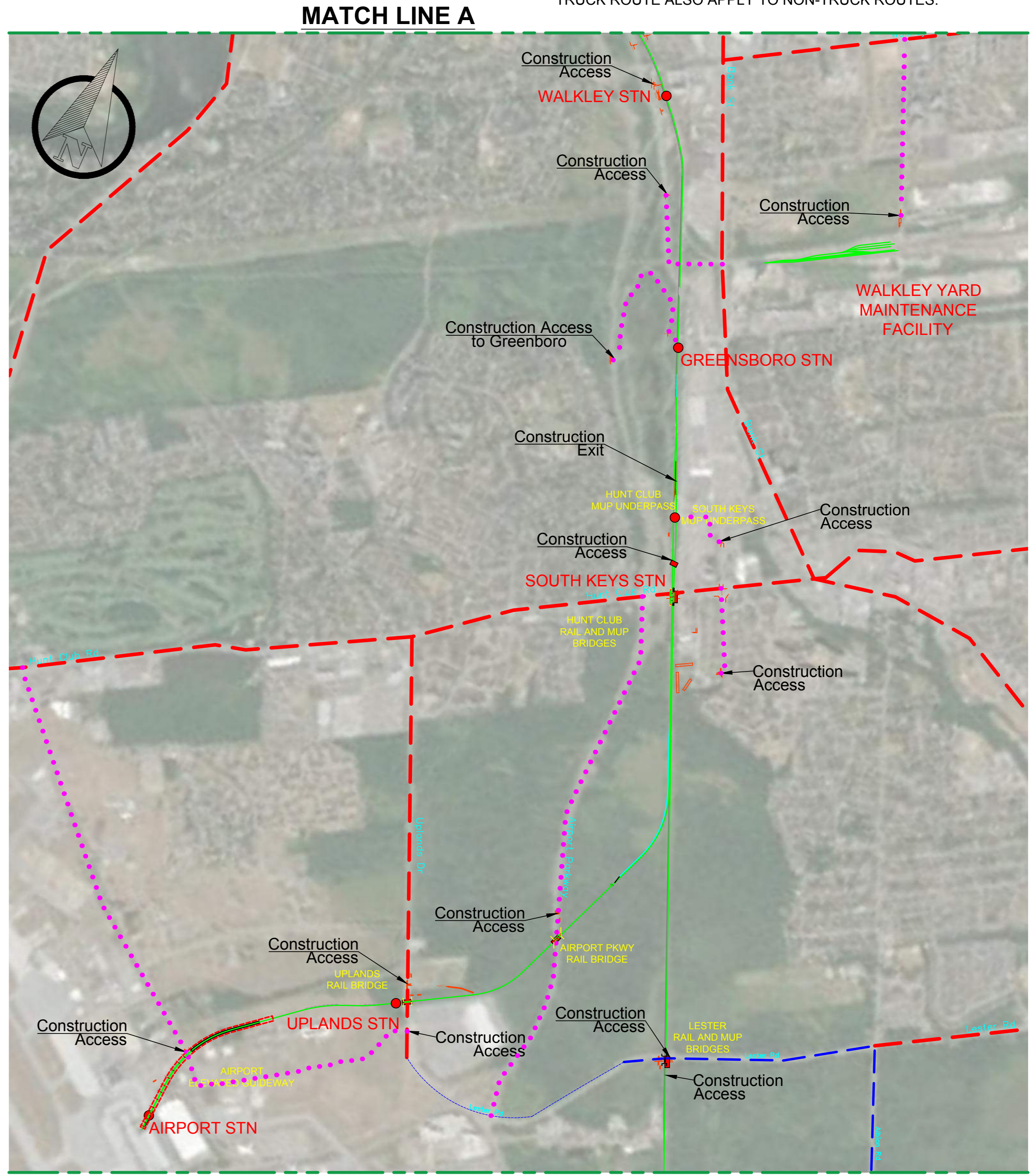


# HAUL ROUTE MAP - MAY 14, 2019 (PAGE 1 OF 2)



MATCH LINE A

- LEGEND**
- STATION
  - BRIDGE/STRUCTURES
  - ELEVATED GUIDEWAY
- TRUCK ROUTES**
- FULL LOADS TRUCK ROUTE
  - RESTRICTED LOADS TRUCK ROUTE  
(RESTRICTED TO 5 TONNES PER AXLE DURING SPRING THAW PERIOD (MID MARCH - MID MAY) EACH YEAR)
  - LOCAL ROUTES ACCESS
  - TRACK RIGHT OF WAY
  - EXCLUSIVE PROJECT CONSTRUCTION VEHICLE ACCESS
- NOTE:**
1. TRUCK ROUTES APPLY TO HEAVY VEHICLES - GROSS WEIGHT > 4.5 TONNES.
  2. HEAVY VEHICLES ARE OBLIGED TO TRAVEL ON TRUCK ROUTES WITH EXCEPTION OF MAKING A DELIVERY TO A PREMISE - SUCH DEVIATION FROM TRUCK ROUTES ARE TO BE MADE FROM THE NEAREST TRUCK ROUTE. RESTRICTIONS ON RESTRICTED LOADS TRUCK ROUTE ALSO APPLY TO NON-TRUCK ROUTES.



MATCH LINE B



# HAUL ROUTE MAP - MAY 14, 2019 (PAGE 2 OF 2)

## LEGEND

- STATION
- BRIDGE/STRUCTURES
- ELEVATED GUIDEWAY

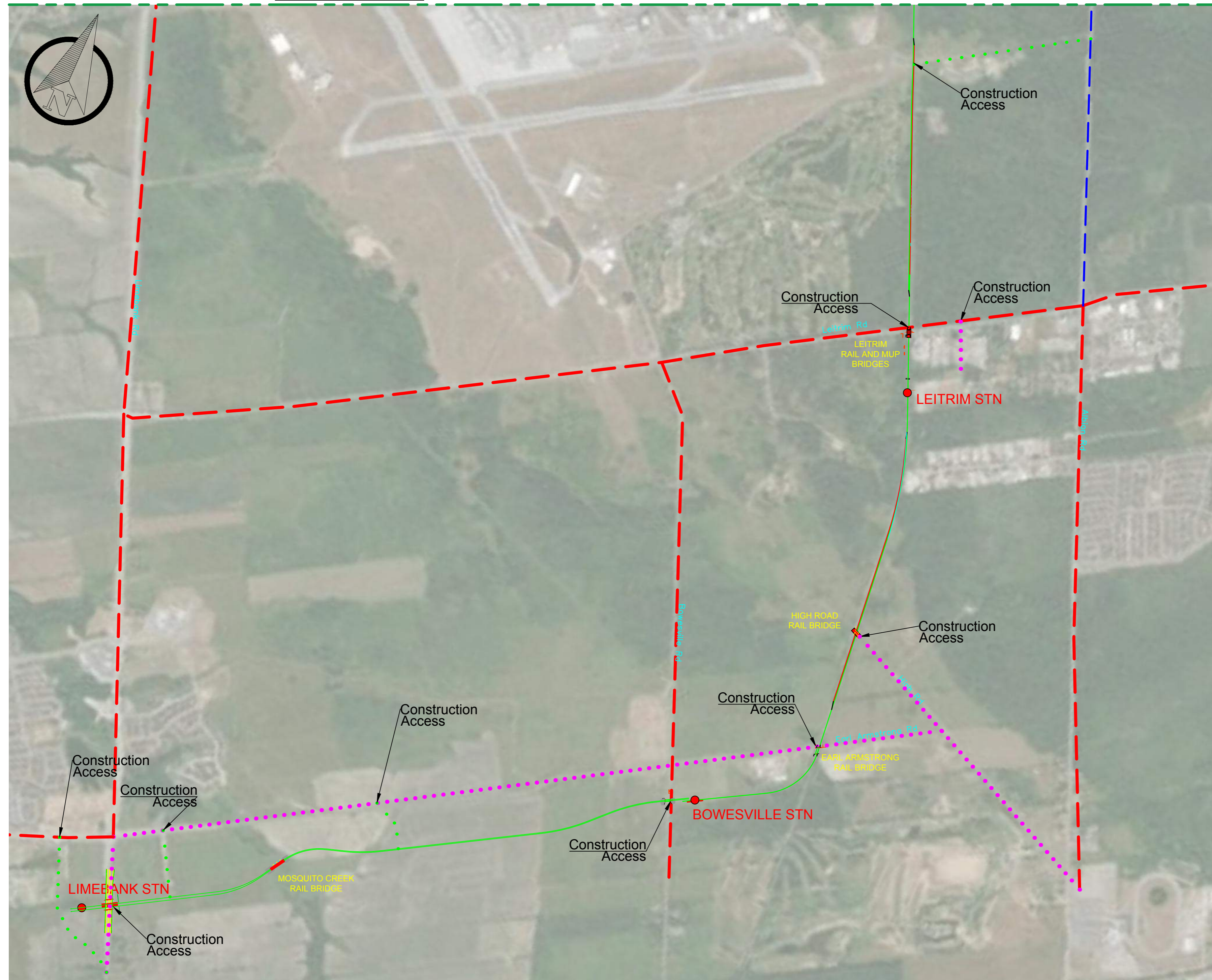
## TRUCK ROUTES


- FULL LOADS TRUCK ROUTE
- RESTRICTED LOADS TRUCK ROUTE  
(RESTRICTED TO 5 TONNES PER AXLE DURING SPRING THAW PERIOD (MID MARCH - MID MAY) EACH YEAR)
- LOCAL ROUTES ACCESS
- TRACK RIGHT OF WAY
- EXCLUSIVE PROJECT CONSTRUCTION VEHICLE ACCESS

## NOTE:

1. TRUCK ROUTES APPLY TO HEAVY VEHICLES - GROSS WEIGHT > 4.5 TONNES.
2. HEAVY VEHICLES ARE OBLIGED TO TRAVEL ON TRUCK ROUTES WITH EXCEPTION OF MAKING A DELIVERY TO A PREMISE - SUCH DEVIATION FROM TRUCK ROUTES ARE TO BE MADE FROM THE NEAREST TRUCK ROUTE. RESTRICTIONS ON RESTRICTED LOADS TRUCK ROUTE ALSO APPLY TO NON-TRUCK ROUTES.

MATCH LINE B



 <b>SNC • LAVALIN</b>	<b>TRILLIUM LINE EXTENSION PROJECT</b> <b>TRAFFIC AND TRANSIT MANAGEMENT PLAN</b>  660373-0000-003-6TAG-0001	<b>Rev.</b>	<b>Date</b>
		02	2021-03-15

# APPENDIX C

## PROJECT SCHEDULES





# Trillium Line South Extension

## 2021 Project Overview

### City of Ottawa Stage 2 Transit Expansion

### Trillium Line South Extension



### Did You Know?

That as an essential service, TransitNEXT's work has been ongoing since the start of the COVID-19 pandemic? The continuation of the Trillium Line South Extension project has meant that, at the end of 2020 TransitNEXT had awarded over 367 orders to 167 local suppliers, representing a local investment of over \$96.8 million during one of the most challenging economic situations in recent history.





# Trillium Line South Extension

## 2021 Project Overview

### Construction Progress

The 2021 construction season will be the busiest to date. This year the TransitNEXT team will mark the completion of several key structures, while work will either start or continue on other new and existing structures.

As of March 2021, work has commenced on all new and existing stations and this year will see significant progress made both on stations and the new Walkley MSF, which will receive its first shipment of trains in summer/fall 2021.

### Construction Look-ahead

Utilities Construction/Relocation (by station/building name)												
	2021											
	J	F	M	A	M	J	J	A	S	O	N	D
Uplands to Airport												
South Keys to Uplands												
Leitrim to Bowesville												
South Keys to Leitrim												
Maintenance & Storage Facility (MSF)												
Bowesville to Limebank												
Greenboro to South Keys												
Mooney's Bay to Walkley												
Carleton to Mooney's Bay												
Walkley to Greenboro												
Carling to Carleton												
Gladstone to Carling												
Bayview to Gladstone												
Dry Utilities (Fiber Optics)												

\*Construction Schedule is subject to change

Guideway Construction												
	2021											
	J	F	M	A	M	J	J	A	S	O	N	D
South Keys Stn. to Uplands Stn.												
Uplands Stn. to Airport Stn.												
Bowesville Stn. to Limebank Stn.												
Southkeys Stn. to Leitrim Stn.												
Leitrim Stn. to Bowesville Stn.												
Greenboro Stn. to South Keys Stn.												
Walkley Stn. to Greenboro Stn.												





# Trillium Line South Extension

## 2021 Project Overview

Mooney's Bay Stn. to Walkley Stn.												
Carleton Stn. to Mooney's Bay Stn.												
Carling Stn. to Carleton Stn.												
Gladstone Stn. to Carling Stn.												
Bayview Stn. to Gladstone Stn.												

\*Construction Schedule is subject to change

Maintenance & Storage Facility (MSF)												
	2021											
	J	F	M	A	M	J	J	A	S	O	N	D
Construction												

\*Construction Schedule is subject to change

Stations Construction												
	2021											
	J	F	M	A	M	J	J	A	S	O	N	D
<b>New Stations, New Extension</b>												
Bowesville Station												
Limebank Station												
South Keys Station												
Leitrim Station												
<b>New Stations, Existing Line</b>												
Walkley Station												
Gladstone Station												
<b>Existing Stations, Existing Line</b>												
Greenboro Station												
Mooney's Bay Station												
Carleton Station												
Carling Station												
Bayview Terminal Station												
<b>New Stations, Airport Link</b>												
Airport Terminal Station												
Uplands Station												

\*Construction Schedule is subject to change





# Trillium Line South Extension

## 2021 Project Overview

Structures Construction												
	2021											
	J	F	M	A	M	J	J	A	S	O	N	D
<b>Existing Structures</b>												
Rail over Rideau River Bridge												
South MUP Underpass at Carleton U												
North MUP Underpass at Carleton U												
Dows Lake Tunnel												
<b>New Structures</b>												
Rail Bridge over Uplands Drive												
Rail Bridge over Airport Parkway												
Rail Bridge over Leitrim Road												
Rail Bridge over Lester Road												
Elevated Guideway to Airport Terminal												
Rail Bridge over Bowesville Road												
Mosquito Creek Rail Bridge												
Rail Bridge over Earl Armstrong Road												
High Road MUP Bridge												
Elevated Guideway at Limebank												
Rail Bridge over Hunt Club Road												
Ellwood Diamond Grade Separation												
Ped. Bridge over Rideau River												







# Trillium Line South Extension

## 2021 Project Overview

### 2021 Traffic & Mobility Impact Look-ahead

Location	Timeline	Traffic & Mobility Impacts
Leitrim Road Rail Bridge	Winter – Summer 2021	Underdeck work and associated activities will require temporary, day-time lane closures during off-peak hours which will affect vehicular traffic.
Earl Armstrong Road Rail Bridge	Winter – Summer 2021	Underdeck work and associated activities will require temporary, day-time lane closures during off-peak hours which will affect vehicular traffic.
Limebank Road Rail Bridge	Spring – Summer 2021	One overnight roadway shutdown for girder installation over Limebank Road. Underdeck work and associated activities will require temporary, day-time lane closures during off-peak hours which will affect vehicular traffic.
Hunt Club Road Rail Bridge	Winter 2021	Caisson drilling in the centre median will require overnight work.
	Spring - Summer 2021	Construction of the rail bridge over Hunt Club will require the installation of a median columns, which will require the nightly lane closures in each direction. Two lanes of day-time traffic in each direction will be maintained at all other times. Erection of bridge girders will require the temporary night-time closure of the road however 1 lane of traffic will be maintained at all times. Underdeck work and associated activities will require temporary, night-time lane closures during off-peak hours which will affect vehicular traffic in each direction.
Rideau River Eastern Pathway (south side of the river)	February – June 30, 2021  AND	To facilitate the construction of the pedestrian bridge over the Rideau River, a section of the Eastern Pathway closed





# Trillium Line South Extension

## 2021 Project Overview

Location	Timeline	Traffic & Mobility Impacts
	September – December 31, 2021	in February 2021, as it travels through an active work zone. During this time pedestrians and cyclists will be detoured along Riverside Drive to Heron Road.
	July & August 2021	During the months of July and August 2021, the Eastern Pathway will re-open for use with flaggers positioned to ensure safe passage for pedestrians and cyclists through the work zone.
Limebank Road	Spring - Fall 2021	Civil work for sewer and water connections and road realignment will require single lane closures.
Leitrim / Gilligan Road	Spring/Summer 2021	Civil work for sewer and water connections and road realignment will require single lane closures.
Hunt Club MUP Crossing	Spring/Summer 2021	Civil work to connect the new MUP will require short-term single lane closures on Millstream Way and Mac Street.
Prince of Wales Drive	Summer 2021	Civil work will require single lane closures, which will include the closure of the bike lane.
Earl Armstrong/ Bowesville Road	Summer 2021	Civil work for sewer and water connections and road realignment will require single lane closures.
Carleton University Station	Summer 2021	Watermain work will require single lane closure on Campus Avenue.
Carling Station	Summer – Fall 2021	Watermain connection will require temporary lane closures of the two right followed by the two left westbound lanes, at which time the crosswalk will also be closed.
Gladstone Station	Summer – Fall 2021	Sanitary and watermain connections will cause temporary closure of westbound lane, west of the rail alignment.

\*Schedule is subject to change



## TRILLIUM OVERALL TRANSPORTATION SUB-STUDIES SCHEDULE FOR INFORMATION ONLY

Report Filing Date	Mar 10th, 2021
Description	Issued for Client Review
Revision Number	0

**Legend**

Not Started	
Ended	

ID	Location	Details	Scheduled Works	Timeframe		Anticipated Submission Date (As of Mar 10th, 2021)	Current Status (As of Mar 10th, 2021)	Comments
				Start	End			
<b>Structures Works</b>								
<b>1 Limebank Elevated Guideway</b>								
3	Limebank Station	Girders	TCP for Girder lift - closure of all 4 lanes (NB and SB)	12-Apr-21	13-Apr-21	12-Mar-21	Not Initiated	
4	Limebank Station	Superstructure	TCP for Formwork, rebar and concrete for superstructure - SB right lane	13-Apr-21	25-Jun-21	12-Mar-21	Not Initiated	
5	Limebank Station	Superstructure	TCP for Formwork, rebar and concrete for superstructure - SB left lane	13-Apr-21	25-Jun-21	12-Mar-21	Not Initiated	
6	Limebank Station	Superstructure	TCP for Formwork, rebar and concrete for superstructure - NB right lane	13-Apr-21	25-Jun-21	12-Mar-21	Not Initiated	
7	Limebank Station	Superstructure	TCP for Formwork, rebar and concrete for superstructure - NB left lane	13-Apr-21	25-Jun-21	12-Mar-21	Not Initiated	
<b>8 Earl Armstrong Bridge</b>								
9	Earl Armstrong Road	Deck	TCP for deck concrete placements (WB)	12-Apr-21	13-Apr-21	22-Mar-21	Not Initiated	
10	Earl Armstrong Road	Deck	TCP for deck concrete placements (EB)	12-Apr-21	12-Apr-21	22-Mar-21	Not Initiated	
11	Earl Armstrong Road	Parapet wall	TCP for Parapet wall concrete placements (WB)	5-May-21	7-May-21	22-Mar-21	Not Initiated	
12	Earl Armstrong Road	Parapet wall	TCP for Parapet wall concrete placements (EB)	5-May-21	7-May-21	22-Mar-21	Not Initiated	
13	Earl Armstrong Road	Deck Formwork removal	TCP for deck formwork removals (WB)	12-May-21	27-May-21	22-Mar-21	Not Initiated	
14	Earl Armstrong Road	Deck Formwork removal	TCP for deck formwork removals (EB)	12-May-21	27-May-21	22-Mar-21	Not Initiated	
<b>15 Leitrim Road Rail Bridge</b>								
16	Leitrim Road	Girders	TCP for Girder lift - closure of WB and EB	17-Feb-21	18-Feb-21	Submitted	Approved	
17	Leitrim Road	Superstructure	TCP for Precast panel, formwork and concrete placement (WB)	19-Feb-21	11-Jun-21	1-Feb-21	Approved	
18	Leitrim Road	Superstructure	TCP for Precast panel, formwork and concrete placement (EB)	19-Feb-21	11-Jun-21	1-Feb-21	Approved	
<b>19 Hunt Club Road Rail Bridge</b>								
20	Hunt Club Road	Conc. Placements	WB closure - TCP for concrete placements	TBC	TBC	Submitted	Approved	
21	Hunt Club Road	Rig Crossing	WB closure - TCP for drill rig crossing	TBC	TBC	Submitted	Approved	
22	Hunt Club Road	Girders	WBL full closure for girder installation	10-May-21	11-May-21	29-Mar-21	Not Initiated	
23	Hunt Club Road	Girders	EBL full closure for girder installation	11-May-21	12-May-21	29-Mar-21	Not Initiated	
24	Hunt Club Road	Superstructure	TCP for Precast panel, formwork and concrete placement (WB/RL)	13-May-21	5-Aug-21	29-Mar-21	Not Initiated	
25	Hunt Club Road	Superstructure	TCP for Precast panel, formwork and concrete placement (WB/LL)	13-May-21	5-Aug-21	29-Mar-21	Not Initiated	
26	Hunt Club Road	Superstructure	TCP for Precast panel, formwork and concrete placement (EB/RL)	13-May-21	5-Aug-21	29-Mar-21	Not Initiated	
27	Hunt Club Road	Superstructure	TCP for Precast panel, formwork and concrete placement (EB/LL)	13-May-21	5-Aug-21	29-Mar-21	Not Initiated	
<b>28 Airport Elevated Guideway</b>								
29	Tracker Private	Span 8	Tracker NB closure - Precast panel, formwork and concrete placement	28-Jan-21	2-Apr-21	8-Feb-21	Approved	
30	Tracker Private	Span 8	Tracker SB closure - Precast panel, formwork and concrete placement	28-Jan-21	2-Apr-21	8-Feb-21	Approved	
31	Silver Star Private	Span 9	Silver star NB closure - Precast panel, formwork and concrete placement	28-Jan-21	2-Apr-21	8-Feb-21	Approved	
32	Silver Star Private	Span 9	Silver star SB closure - Precast panel, formwork and concrete placement	28-Jan-21	2-Apr-21	8-Feb-21	Approved	
33	Paul Benoit Driveway	Span 11	Paul Benoit EB closure - Precast panel, formwork and concrete placement	28-Jan-21	2-Apr-21	8-Feb-21	Approved	
34	Paul Benoit Driveway	Span 11	Paul Benoit WB closure - Precast panel, formwork and concrete placement	28-Jan-21	2-Apr-21	8-Feb-21	Approved	
<b>35 Ellwood Diamond Rail Bridge</b>								
36	Ellwood Diamond (Transitway)	Demolition	TCP for existing bridge demolition	26-Mar-21	29-Mar-21	1-Feb-21	Submitted	
37	Ellwood Diamond (Transitway)	Girder	TCP for Span 4, 5 and 6 girder installation	11-Jun-21	14-Jun-21	1-May-21	Not Initiated	
38	Ellwood Diamond (Transitway)	Deck work	TCP for flagging operations	14-Jun-21	23-Jul-21	1-May-21	Not Initiated	
<b>39 Pedestrian Bridge over Rideau River</b>								
40	Rideau River Eastern Pathway	Pathway flagging	TCP for Flagging the eastern pathway on July and August, 2021	1-Apr-21	TBD	12-Feb-21	Submitted	
41	Rideau River Eastern Pathway	Pathway closure	TCP for Full closure of the eastern pathway	1-Apr-21	TBD	12-Feb-21	Submitted	
<b>42 Existing Rail Bridge Over Rideau River</b>								
43	University Drive	Span 5	TCP for Traffic Signals for Span 5 works	9-Apr-21	14-May-21	20-Mar-21	Revisions Req'd	
44	University Drive	Span 5	TCP for Traffic Signals for Span 5 works	9-Apr-21	30-Jun-21	20-Mar-21	Revisions Req'd	
45	University Drive	Pedestrian detour	Pedestrian detour TCP for closure of the sidewalk along Rideau River	9-Apr-21	24-Sep-21	20-Mar-21	Approved	
<b>46 Civil Works</b>								
<b>47 Limebank Road</b>								
48	Limebank Road	Sewer connections	Concept plan for NB crossover		Spring 2021	8-Mar-21	Not Initiated	
49	Limebank Road	Sewer connections	TCP for NB crossover		Spring 2021	8-Mar-21	Not Initiated	
50	Limebank Road	Road realignmemnt	TCP for NB outside lane closure		Fall 2021	1-May-21	Not Initiated	
51	Limebank Road	Road realignmemnt	TCP for NB inside lane closure		Fall 2021	1-May-21	Not Initiated	
52	Limebank Road	Road realignmemnt	TCP for SB outside lane closure		Fall 2021	1-May-21	Not Initiated	
53	Limebank Road	Road realignmemnt	TCP for SB inside lane closure		Fall 2021	1-May-21	Not Initiated	
<b>54 Earl Armstrong Road</b>								
55	Earl Armstrong Road	Culvert work	WB lane closure - in front of the park and ride facility	1-Aug-21	TBD	1-Apr-21	Not Initiated	
56	Earl Armstrong Road	Culvert work	EB lane closure - in front of the park and ride facility	1-Aug-21	TBD	1-Apr-21	Not Initiated	
<b>57 Earl Armstrong/Bowesville Road</b>								
58	Earl Armstrong/Bowesville Rd	Road work	TCP for NB Bowesville lane closure	1-Aug-21	TBD	1-Apr-21	Not Initiated	
59	Earl Armstrong/Bowesville Rd	Road work	TCP for SB Bowesville lane closure	1-Aug-21	TBD	1-Apr-21	Not Initiated	
60	Earl Armstrong/Bowesville Rd	Road work	TCP for WB Earl Armstrong closure	1-Aug-21	TBD	1-Apr-21	Not Initiated	
61	Earl Armstrong/Bowesville Rd	Road work	TCP for EB Earl Armstrong closure	1-Aug-21	TBD	1-Apr-21	Not Initiated	
62	Earl Armstrong/Bowesville Rd	Road work	TCP for intersection work - flaggers at 4 corners of the intersection	1-Aug-21	TBD	1-Apr-21	Not Initiated	
63	Culvert 507	Culvert	TCP for NB lane closure on Bowesville road - North of Earl Armstrong	1-Apr-21	TBD	1-Mar-21	Not Initiated	
<b>64 Lester Rail Bridge</b>								
65	Lester Road	MUP crossing	TCP for EB Lane Closure - Flagging operations		Spring 2022	TBD	Not Initiated	
66	Lester Road	MUP crossing	TCP for WB Lane Closure - Flagging operations		Spring 2022	TBD	Not Initiated	
<b>67 Leitrim Rail Bridge</b>								
68	Leitrim Road	MUP crossing	TCP for EB Lane Closure - Flagging operations		Spring 2022	TBD	Not Initiated	
69	Leitrim Road	MUP crossing	TCP for WB Lane Closure - Flagging operations		Spring 2022	TBD	Not Initiated	
70	Gilligan Road	Sewer connections	TCP for NB lane closure	1-Jun-21	TBD	1-May-21	Not Initiated	



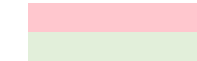
71	Gilligan Road	Sewer connections	TCP for SB lane closure	1-Jun-21	TBD	1-May-21	Not Initiated
72	Gilligan Road	Road realignment	TCP for NB lane closure	1-Jun-21	TBD	1-May-21	Not Initiated
73	Gilligan Road	Road realignment	TCP for SB lane closure	1-Jun-21	TBD	1-May-21	Not Initiated
74	Gilligan Road	Sidewalk/ Asphalt	TCP for SB lane closure for sidewalk installation	1-Jun-21	TBD	1-May-21	Not Initiated
75	Leitrim Road	Sidewalk/ Asphalt	TCP for EB closure near intersection - may require Gilligan Road lane closures as well	1-Jun-21	TBD	1-May-21	Not Initiated
<b>Hunt Club Rail Bridge</b>							
77	Hunt Club Bridge	RSS wall	TCP for North sidewalk closure	1-May-21	TBD	1-Apr-21	Not Initiated
78	MUP Crossing to Millstream way	MUP Connection	TCP for SB lane closure - Millstream and Crosscut intersection	1-Jun-21	TBD	1-May-21	Not Initiated
79	MUP Crossing to Millstream way	MUP Connection	TCP for NB Mac Street lane closure	1-Jun-21	TBD	1-May-21	Not Initiated
80	MUP Crossing to Millstream way	MUP Connection	TCP for SB Mac Street lane closure	1-Jun-21	TBD	1-May-21	Not Initiated
<b>Walkley Road</b>							
82	Walkley road sidewalk installation	Sidewalk closure	New sidewalks to connect Walkley Station to Walkley Road	1-Oct-21	TBD	TBD	Not Initiated
<b>Airport Parkway</b>							
84	Airport Parkway	Service road connection	TCP for NB lane closure	Spring 2022		TBD	Not Initiated
<b>Other - Prince of Wales Watermain</b>							
86	Princes of Wales Drive (South of Carling Ave)	Watermain installation	TCP for NB w/ bike lane closure	1-Aug-21	TBD	1-May-21	Not Initiated
87	Princes of Wales Drive (South of Carling Ave)	Watermain installation	TCP for SB w/ bike lane closure	1-Aug-21	TBD	1-May-21	Not Initiated
<b>Station Works</b>							
<b>Limbank Station</b>							
90	Limbank Station	Utilities	TCP for Watermain and sewer connection (storm and sanitary)	1-May-21	TBD	8-Mar-21	Not Initiated
91	Limbank Station	Utilities	TCP for Duct bank crossing	1-May-21	TBD	8-Mar-21	Not Initiated
92	Limbank Station		OC Transpo Pre-Testing	1-Mar-22			
<b>Bowesville Station</b>							
94	Bowesville Station	Utilities	Watermain - NB Bowesville road lane closure / shoulder work	1-Aug-21	TBD	1-Apr-21	Not Initiated
95	Bowesville Station	Utilities	Watermain - Intersection for Earl and Bowesville	1-Aug-21	TBD	1-Apr-21	Not Initiated
96	Bowesville Station	Utilities	TCP for Watermain and sewer connections - EB Earl Armstrong Road	1-Aug-21	TBD	1-Apr-21	Not Initiated
97	Bowesville Station	Utilities	TCP for Watermain and sewer connections - WB Earl Armstrong Road	1-Aug-21	TBD	1-Apr-21	Not Initiated
98	Bowesville Station		OC Transpo Pre-Testing	1-Jun-22			
<b>Leitrim Station</b>							
100	Leitrim Station	Utilities	Watermain - NB Gilligan Road at the OC Entrance	1-Jun-21	TBD	1-May-21	Not Initiated
101	Leitrim Station	Utilities	Watermain - SB Gilligan Road at the OC Entrance	1-Jun-21	TBD	1-May-21	Not Initiated
102	Leitrim Station	Utilities	Watermain - OC Park and ride - Near the corner WB	1-Jun-21	TBD	1-May-21	Not Initiated
103	Leitrim Station	Utilities	Watermain - OC Park and ride - Near the corner EB	1-Jun-21	TBD	1-May-21	Not Initiated
104	Leitrim Station		OC Transpo Pre-Testing	1-Jun-22			
<b>South Keys Station</b>							
106	South Keys Station	Utilities	Watermain - NB AP from EB Hunt Club lane reduced width	3-May-21	4-Jun-21	12-Mar-21	Not Initiated
107	South Keys Station	Utilities	Watermain - NB on ramp from Hunt Club - 2 stages for closure	3-May-21	4-Jun-21	12-Mar-21	Not Initiated
108	South Keys Station	Utilities	Watermain - Pathway closure for open trench work	3-May-21	4-Jun-21	12-Mar-21	Not Initiated
<b>Carleton Station</b>							
110	Campus Avenue	Utilities	Watermain - Campus Ave left lane closure	Summer 2021		1-May-21	Not Initiated
111	Campus Avenue	Utilities	Watermain - Campus Ave right lane closure	Summer 2021		1-May-21	Not Initiated
<b>Dow's Lake Tunnel</b>							
113	Dow's Lake Pumphouse	Access	Access TCP - show signs on the pathway in all directions	21-Mar-21	21-Sep-21	12-Mar-21	Not Initiated
<b>Carling Station</b>							
115	Carling Station	Utilities	TCP for Sanitary connection - Pathway closed? Reduced width	21-Aug-21	21-Nov-21	1-Aug-21	Not Initiated
116	Carling Station	Utilities	TCP for Sanitary connection - Adeline street construction zone	21-Aug-21	21-Nov-21	1-Aug-21	Not Initiated
117	Carling Station	Utilities	TCP for WM connection - WB 2 x right lanes closure	21-Aug-21	21-Nov-21	1-Aug-21	Not Initiated
118	Carling Station	Utilities	TCP for WM connection - WB 2 x left lanes closed and crosswalk closed	21-Aug-21	21-Nov-21	1-Aug-21	Not Initiated
<b>Gladstone Station</b>							
120	Gladstone Station	Utilities	TCP for Sanitary works - WB lane closure West of the rail alignment	21-Aug-21	21-Oct-21	1-Jul-21	Not Initiated
121	Gladstone Station	Utilities	TCP for Watermain works - WB lane closure West of the rail alignment	21-Aug-21	21-Oct-21	1-Jul-21	Not Initiated
<b>Bayview Station</b>							
123	Bayview Station	MUP	TCP for MUP Connections	21-Feb-21	21-Mar-21	Submitted	Submitted
124	Bayview Station	Structure Erection	TCP for Trinity Ped bridge construction - Overnight closure	21-Oct-21	21-Nov-21	1-Oct-21	Not Initiated
125	Bayview Station	Trackwork	TCP for trackworks north side of tracks	TBD		TBD	Not Initiated
<b>Uplands Station</b>							
127	Uplands Station		OC Transpo Pre-Testing	1-Oct-22			

## TRILLIUM Transportation Sub-Studies Schedule Hunt Club Area - For Information Only

Report Filing Date	March 10th, 2021
Description	Issued for Client Review
Revision Number	1

**Legend**


Not Started  
Ended



ID	Scheduled Works	File Reference	PA Transportation Studies	Timeframe		TransitNEXT Status (As of Mar 10th, 2021)		City of Ottawa Status (As of Mar 10th 2021)		Comments
				Start	End	Status	Date	Status	Date	
1	<b>Phase 0 - Preparatory Studies</b>									
2	<b>Traffic Memorandum</b>									
3	Hunt Club Road Traffic Impact Memorandum	660373-3BHM-003-4TEN-0001	Traffic Management Study	N/A	N/A	Complete	2019-11-13	Submitted to the City	2019-11-13	
4	<b>Site-Specific Traffic and Transit Management Plan</b>									
5	SSTMP for Hunt Club Rail Bridge and MUP Structures	660373-3BHC-003-6TAG-0001	Site-Specific Traffic and Transit Management Plan	N/A	N/A	Complete	2021-01-20	Submitted to the City	2021-01-20	
6	<b>Phase 1 - Site Preparation</b>									
7	<b>Temporary Construction Access</b>									
8	Temporary Access from Airport Parkway Off-Ramp to H-C	660373-4CSL-003-4TDK-TCP-0003	Traffic Control Plan	15-Jun-20	TBC	Complete	2020-06-09	Cancelled	2020-06-15	
9	<b>Geotechnical Works</b>									
10	Boreholes on the NB Airport Parkway Off-Ramp to H-C	660373-4CSU-003-4TDK-TCP-0001	Traffic Control Plan	02-Jul-20	03-Jul-20	Complete	2020-06-29	Closed	2020-07-03	
11	<b>Hunt Club Road MH Inspections</b>									
12	MH Inspection at median and EB left lane	660373-4CSU-003-4TDK-TCP-0002	Traffic Control Plan	20-Jul-20	20-Jul-20	Complete	2020-07-14	Closed	2020-07-20	
13	MH Inspection - WB Sidewalk	660373-4CSU-003-4TDK-TCP-0003	Traffic Control Plan	20-Jul-20	20-Jul-20	Complete	2020-07-14	Closed	2020-07-20	
14	MH Inspection - EB Bus Lane	660373-4CSU-003-4TDK-TCP-0005	Traffic Control Plan	20-Jul-20	20-Jul-20	Complete	2020-07-08	Closed	2020-07-20	
15	<b>Hydro Works Northbound, Impacts on Slow Lane and Ramps</b>									
16	Pedestrian Detour Plan from Airport Pkwy to East of Transitway NB On-Ramp	660373-4CSU-003-4TDK-TCP-0009	Traffic Control Plan	05-Oct-20	17-Oct-20	Complete	2020-08-19	Closed	2020-10-23	
17	WB Slow Lane, Transitway NB On-Ramp and SB Off-Ramp Closures	660373-4CSU-003-4TDK-TCP-0010	Traffic Control Plan	11-Oct-20	16-Oct-20	Complete	2020-08-19	Closed	2020-10-16	
18	PVMS for Airport Parkway NB On-Ramp (East) Closure	660373-4CSU-003-4TDK-TCP-0013	Traffic Control Plan	11-Oct-20	21-Oct-20	Complete	2020-08-19	Closed	2020-10-21	
19	WB Inside Lane Closure	660373-4CSU-003-4TDK-TCP-0028	Traffic Control Plan	05-Oct-20	23-Oct-20	Complete	2020-09-25	Closed	2020-10-23	
20	Airport Parkway NB On-Ramp Closure	660373-4CSU-003-4TDK-TCP-0029	Traffic Control Plan	18-Oct-20	21-Oct-20	Complete	2020-09-25	Closed	2020-10-21	
21	Sidewalk East of Transitway NB On-Ramp Closure and Works on the Airport Pkwy NB On-Ramp Grass Area	660373-4CSU-003-4TDK-TCP-0032	Traffic Control Plan	25-Nov-20	27-Nov-20	Complete	2020-11-23	Closed	2020-11-27	
22	Pedestrian Detour Plan from Trillium Rail Line to East of Transitway NB On-Ramp	660373-4CSU-003-4TDK-TCP-0033	Traffic Control Plan	25-Nov-20	27-Nov-20	Complete	2020-11-23	Closed	2020-11-27	
23	<b>South Hydro Works</b>									
24	Pedestrian Detour Plan from Airport Pkwy to Bridle Path Drive	660373-4CSU-003-4TDK-TCP-0016	Traffic Control Plan	15-Sep-20	02-Oct-20	Complete	2020-09-14	Closed	2020-10-02	
25	EB Slow Lane and Sidewalk Closures	660373-4CSU-003-4TDK-TCP-0018	Traffic Control Plan	15-Sep-20	22-Sep-20	Complete	2020-09-14	Closed	2020-09-22	
26	EB Slow Lane and Airport Parkway Off-Ramp Closures	660373-4CSU-003-4TDK-TCP-0021	Traffic Control Plan	24-Sep-20	28-Sep-20	Complete	2020-09-14	Closed	2020-09-28	
27	Detour for Airport Parkway NB Off-Ramp Closure	660373-4CSU-003-4TDK-TCP-0022	Traffic Control Plan	24-Sep-20	28-Sep-20	Complete	2020-09-14	Closed	2020-09-28	
28	Airport Parkway NB Off-Ramp at H-C Closure - Walkley Detour	660373-4CSU-003-4TDK-TCP-0023	Traffic Control Plan	24-Sep-20	28-Sep-20	Complete	2020-09-14	Closed	2020-09-28	
29	H-C EB Slow Lane and Sidewalk Closures	660373-4CSU-003-4TDK-TCP-0025	Traffic Control Plan	15-Sep-20	02-Oct-20	Complete	2020-09-14	Closed	2020-10-02	
30	H-C EB Slow Lane, Transitway Off-Ramp and Sidewalk Closures	660373-4CSU-003-4TDK-TCP-0027	Traffic Control Plan	24-Sep-20	01-Nov-20	Complete	2020-09-14	Closed	2020-11-01	
31	Hydro Works in the EB Grass Area, and Sidewalk Closure East of Mac Street	660373-4CSU-003-4TDK-TCP-0030	Traffic Control Plan	11-Nov-20	27-Nov-20	Complete	2020-11-10	Implemented	2020-11-27	
32	Pedestrian Detour for Sidewalk Closure, East of Mac Street	660373-4CSU-003-4TDK-TCP-0031	Traffic Control Plan	11-Nov-20	27-Nov-20	Complete	2020-11-10	Implemented	2020-11-27	
33	<b>Rogers Works</b>									
34	Airport NB On-Ramp Closure	Not available yet	Traffic Control Plan	2021-03-20 (Tentative)	TBC	Not Initiated	2021-01-20			
35	NB Transitway On-Ramp and Off-Ramp Closure	Not available yet	Traffic Control Plan	2021-03-20 (Tentative)	TBC	Not Initiated	2021-01-20			
36	<b>Watermain Works</b>									
37	Airport Parkway NB On-Ramp Closures	Not available yet	Traffic Control Plan	2021-03-12 (Tentative)	TBC	Not Initiated	2021-01-20			
38	Lane closure on the Airport parkway off-ramp NB at Hunt Club road	Not available yet	Traffic Control Plan	2021-03-12 (Tentative)	TBC	Not Initiated	2021-01-20			
39	<b>Phase 2 - Bridge Substructure Works</b>									
40	<b>North Caisson Works</b>									
41	Flagger to be used for Construction Access on the Transitway Off-Ramp to H-C - North-East Caisson Works	660373-3BHC-003-4TDK-TCP-0004	Traffic Control Plan	12-Oct-20	23-Oct-20	Complete	2020-10-06	Closed	2020-10-23	
42	Conceptual Drawing For Sidewalk Extension near Bus Stop (#8608)	660373-3BHC-003-6TDD-1002	Detailed Conceptual Design	11-Jan-21	15-Feb-21	Complete	2021-01-07	Approved	2021-01-07	
43	<b>Central Caisson Works</b>									
44	TCB Plan for H-C Central Caissons Works	660373-3BHC-003-6TDD-1000	Detailed Road Design	10-Nov-20	30-Nov-21	Complete	2020-11-09	Implemented	2020-11-30	
45	Conceptual Pavement Marking Plan for H-C Central Caisson Construction	660373-3BHC-003-6TDD-1001	Detailed Road Design	10-Nov-20	30-Nov-21	Complete	2020-11-09	Implemented	2020-11-30	
46	EB Left Lane Closure for TCB Installation	660373-3BHC-003-4TDK-TCP-0001	Traffic Control Plan	10-Nov-20	30-Nov-21	Complete	2020-11-09	Closed	2020-11-10	
47	EB and WB Left Lane Reduction for Night Works	660373-3BHC-003-4TDK-TCP-0003	Traffic Control Plan	10-Nov-20	30-Nov-20	Complete	2020-11-09	Closed	2020-11-30	

48	EB Bus Lane and Slow Lane Reduction for EB Line Painting & Bus/Bike Lane Paint Removal	660373-3BHC-003-4TDK-TCP-0006	Traffic Control Plan	09-Nov-20	10-Nov-20	Complete	2020-11-09	Closed	2020-11-10	
49	TCP Hunt Club Road EB Lane Closure for Drill Rig Crossing	660373-3BHC-003-4TDK-TCP-0010	Traffic Control Plan	27-Jan-21	27-Jan-21	Complete	2021-01-15	Closed	2021-01-28	
50	TCP Advance Notice for Ramp Closures at Hunt Club Road - for Drill Rig Crossing	660373-3BHC-003-4TDK-TCP-0012	Traffic Control Plan	25-Jan-21	26-Jan-21	Complete	2021-01-15	Closed	2021-01-28	
51	TCP Hunt Club Road EB Lane Closure for Concrete Placement in the Median	660373-3BHC-003-4TDK-TCP-0013	Traffic Control Plan	30-Jan-21	07-Feb-21	Complete	2021-01-18	Submitted for Approval	2021-01-18	
52	TCP Advance Notice for Ramp Closures at Hunt Club Road - for Concrete Placement in the Median	660373-3BHC-003-4TDK-TCP-0014	Traffic Control Plan	30-Jan-21	13-Feb-21	Complete	2021-01-18	Submitted for Approval	2021-01-18	
53	TCP Hunt Club Road EB Lane Closure for Concrete Placement in the Median (Contingency Dates)	660373-3BHC-003-4TDK-TCP-0015	Traffic Control Plan	03-Feb-21	10-Feb-21	Complete	2021-01-18	Submitted for Approval	2021-01-18	
54	TCP Advance Notice for Ramp Closures at Hunt Club Road - for Concrete Placement in the Median (Contingency Dates)	660373-3BHC-003-4TDK-TCP-0016	Traffic Control Plan	03-Feb-21	10-Feb-21	Complete	2021-01-18	Submitted for Approval	2021-01-18	
55	TCP Hunt Club Road EB Lane Closure for Drill Rig Crossing (Back to North Work Area)	660373-3BHC-003-4TDK-TCP-0017	Traffic Control Plan	17-Feb-21	18-Feb-21	Complete	2021-01-26	Approved	2021-01-26	
56	TCP Advance Notice for Ramp Closures at Hunt Club Road - for Drill Rig Crossing (Back to North Work Area)	660373-3BHC-003-4TDK-TCP-0018	Traffic Control Plan	17-Feb-21	18-Feb-21	Complete	2021-01-26	Approved	2021-01-26	
	TCP on Hunt Club Road - Settlement Monitoring of WB Lane near the Median Caisson Work	660373-3BHC-003-4TDK-TCP-0019	Traffic Control Plan	TBC	TBC	Complete	2021-03-02	Submitted for Approval	2021-03-02	
57	<b>South Caisson Works</b>									
58	South Caisson Works - Construction Access from the Bus Loop	660373-3BHC-003-4TDK-TCP-0007	Traffic Control Plan	12-Jul-05	24-Dec-20	Complete	2020-11-19	Submitted for Approval	2020-11-20	
59	Pedestrian Detour from Airport Pkwy NB Off-Ramp to Transitway Off-Ramp	660373-3BHC-003-4TDK-TCP-0008	Traffic Control Plan	26-Nov-20	2021-02-15 (TBC)	Complete	2020-11-24	Implemented	2020-11-26	
60	Flaggers used on the Bus Loop too assist construction access	660373-3BHC-003-4TDK-TCP-0009	Traffic Control Plan	07-Dec-20	24-Dec-20	Complete	2020-12-01	Closed	2020-12-24	
61	Pedestrian Detour for Sidewalk Closure, for South Caisson Works	660373-3BHC-003-4TDK-TCP-0010	Traffic Control Plan	04-Dec-20	2021-02-15 (TBC)	Complete	2020-12-04	Implemented	2020-12-04	
62	EB Slow Lane Reduction for Crane Pad Works	660373-3BHC-003-4TDK-TCP-0011	Traffic Control Plan	07-Dec-20	08-Dec-20	Complete	2020-12-07	Closed	2020-12-08	
63	<b>Phase 3 - Bridge Superstructure Works</b>									
64	<b>Structural Works</b>									
65	<b>North Pier Cap Construction</b>									
66	Sidewalk closure from Airport Pkwy NB On-Ramp to Transitway Off-Ramp	Not available yet	Traffic Control Plan	2021-04-30 (Tentative)	TBC	Not Initiated	2021-01-20			
67	Slow lane closure for Column Concrete Pour	Not available yet	Traffic Control Plan	2021-04-01 (Tentative)	TBC	Not Initiated	2021-01-20			
68	Slow Lane closure for Pier Cap Concrete Pour	Not available yet	Traffic Control Plan	2021-04-14 (Tentative)	TBC	Not Initiated	2021-01-20			
69	<b>South Pier Cap Construction</b>									
70	Sidewalk Closure from Airport Pkwy NB Off-Ramp to Transitway Off-Ramp	Not available yet	Traffic Control Plan	2021-02-15 (Tentative)	TBC	Not Initiated	2021-01-20			
71	Slow lane closure for Column Concrete Pour	Not available yet	Traffic Control Plan	2021-02-12 (Tentative)	TBC	Not Initiated	2021-01-20			
72	Slow Lane closure for Pier Cap Concrete Pour	Not available yet	Traffic Control Plan	2021-02-26 (Tentative)	TBC	Not Initiated	2021-01-20			
73	<b>Centre Pier Cap Construction</b>									
74	Left lane closure for Column Concrete Pour	Not available yet	Traffic Control Plan	2021-04-05 (Tentative)	TBC	Not Initiated	2021-01-20			
75	Left Lane closure for Pier Cap Concrete Pour	Not available yet	Traffic Control Plan	2021-04-19 (Tentative)	TBC	Not Initiated	2021-01-20			
76	<b>Girder Installation Over Traffic</b>									
77	WB Traffic Lane Closure for WB Girders Installation	Not available yet	Traffic Control Plan	2021-05-01 (Tentative)	TBC	Not Initiated	2021-01-20			
78	EB Traffic Lane Closure for EB Girders Installation	Not available yet	Traffic Control Plan	2021-05-01 (Tentative)	TBC	Not Initiated	2021-01-20			
79	<b>Precast Deck Panels Lift over EB Traffic</b>									
80	EB Lane Closure for Slab Precast Lifts	Not available yet	Traffic Control Plan	2021-06-01 (Tentative)	TBC	Not Initiated	2021-01-20			
81	<b>Precast Deck Panels Lift over WB Traffic</b>									
82	WB Slow Lane Closure for Slab Precast Lifts	Not available yet	Traffic Control Plan	2021-07-01 (Tentative)	TBC	Not Initiated	2021-01-20			
83	<b>Preparation of Bridge Parapet Walls</b>									
84	WB Slow Lane Closure for Concrete Pour (TCP not confirmed yet)	Not available yet	Traffic Control Plan	2021-07-01 (Tentative)	TBC	Not Initiated	2021-01-20			
85	EB Slow Lane Closure for Concrete Pour (TCP not confirmed yet)	Not available yet	Traffic Control Plan	2021-07-01 (Tentative)	TBC	Not Initiated	2021-01-20			
86	<b>Civil Works</b>									
87	<b>MSE Wall Panels Installation and Backfill</b>									
88	WB Sidewalk Closure from Airport Pkwy NB On-Ramp to Transitway Off-Ramp	Not available yet	Traffic Control Plan	2020-04-03 (Tentative)	2021-07-01 (Tentative)	Not Initiated	2021-01-20			
89	EB Sidewalk Closure from Airport Pkwy NB On-Ramp to Transitway Off-Ramp	Not available yet	Traffic Control Plan	2020-04-03 (Tentative)	2021-07-01 (Tentative)	Not Initiated	2021-01-20			
90	<b>MUP Closure to the North of Hunt Club</b>									
91	Existing MUP detour for MUP reconfiguration	Not available yet	Traffic Control Plan	2021-07-05 (Tentative)	TBC	Not Initiated	2021-01-20			
92	<b>Phase 4 - Final Works</b>									
93	Hunt Club TCB removal and reinstatement to original conditions	Not available yet	Traffic Control Plan	2021-12-01 (Tentative)	TBC	Not Initiated	2021-01-20			
94	Construction of the Pathway connecting Mac Street and Bus Loop	Not available yet	Traffic Control Plan	2021-08-15 (Tentative)	TBC	Not Initiated	2021-01-20			



 <b>SNC • LAVALIN</b>	<b>TRILLIUM LINE EXTENSION PROJECT</b> <b>TRAFFIC AND TRANSIT MANAGEMENT PLAN</b>  660373-0000-003-6TAG-0001	<b>Rev.</b>	<b>Date</b>
		02	2021-03-15

## APPENDIX D

# CURRENT ROSTER OF TRILLIUM TRANSPORTATION SUB-STUDIES (LIVING DOCUMENT)

# Trillium - TIA & RSA Submissions

Type	Deliverable Title	Document No.	Anticipated Submission Date	Status
TIA	Airport Station TIA	660373-1APS-003-6TER-0001	2020-08-25	2. Submitted to AHJ
TIA	Bayview Station TIA	660373-1BVC-003-6TER-0001	2020-08-21	3. Submitted Internally
RSA	Bowesville Park and Ride Facility		2019-10-30	2. Submitted to AHJ
RSA	Bowesville Park and Ride Facility and Adjacent Roads (Per Updated Design)		2020-02-07	5. Overdue
RSA	Bowesville Road (520m; ## TCS)		2019-10-30	2. Submitted to AHJ
TIA	Bowesville Station TIA	660373-1BWC-003-6TER-0001	2020-09-11	2. Submitted to AHJ
TIA	Carleton Station TIA	660373-1CRC-003-6TER-0001	2020-09-29	2. Submitted to AHJ
TIA	Carling Station TIA	660373-1CLC-003-6TER-0001	2020-10-27	2. Submitted to AHJ
RSA	Clear Skies Parkway (150m; # TCS) (Airport)		2019-10-30	2. Submitted to AHJ
RSA	Collector Road D (200m; # TCS) (Limebank)		2019-10-30	2. Submitted to AHJ
RSA	Connector Road (300m; # TCS) (Limebank)		2019-10-30	2. Submitted to AHJ
TIA	Earl Armstrong and Bowesville Intersection Reconfiguration TIA		2020-03-30	5. Overdue
RSA	Earl Armstrong Road (900m; ## TCS)		2019-10-30	2. Submitted to AHJ
TIA	Gladstone Station TIA	660373-1GSC-003-6TER-0001	2020-10-27	2. Submitted to AHJ
TIA	Greenboro Station TIA	660373-1GBC-003-6TER-0001	2020-10-06	2. Submitted to AHJ
RSA	Leitrim Park and Ride Facility		2019-10-30	2. Submitted to AHJ
TIA	Leitrim Station TIA	660373-1LTC-003-6TER-0001	2020-09-11	2. Submitted to AHJ
RSA	Limebank Road (500m; # TCS)		2019-10-30	2. Submitted to AHJ
TIA	Limebank Station TIA	660373-1LBC-003-6TER-0001	2020-08-21	2. Submitted to AHJ
RSA	Main Street (300m; ## TCS)		2019-10-30	2. Submitted to AHJ
TIA	Mooney's Bay TIA	660373-1MBC-003-6TER-0001	2020-09-01	3. Submitted Internally
TIA	MSF TIA	660373-2WYM-003-6TER-0001	2020-03-30	2. Submitted to AHJ
TIA	South Keys Station TIA	660373-1SKC-003-6TER-0001	2020-08-21	2. Submitted to AHJ
TIA	Uplands Station TIA	660373-1ULC-003-6TER-0001	2020-08-21	2. Submitted to AHJ
TIA	Walkley Station TIA	660373-1WKC-003-6TER-0001	2020-09-29	2. Submitted to AHJ

# Trillium - SSTTMP Submissions

Type	Deliverable Title	Document No.	Anticipated Submission Date	Status
STTMP	Carling Station Tree Cutting SSTTMP	660373-1CLC-003-6TAG-0001	2020-09-11	1. Accepted
STTMP	Gladstone MUP Detour and Access	660373-1GSC-003-6TAG-0001	2020-06-01	1. Accepted
STTMP	Airport Parkway Rail Bridge	660373-3BAP-003-6TAG-0001	2020-09-11	2. Submitted to AHJ
STTMP	Mooney's Bay Station Tree Cutting		2020-01-23	2. Submitted to AHJ
STTMP	Greenboro Station Access	660373-1GBC-003-6TAG-0001	2020-09-18	2. Submitted to AHJ
STTMP	Hunt Club Rail & MUP Structures	660373-3BHC-003-6TAG-0001	2020-09-18	2. Submitted to AHJ
STTMP	Limebank Station SSTTMP	660373-1LBC-003-6TAG-0001	2020-09-18	2. Submitted to AHJ
STTMP	Walkley Station SSTTMP	660373-1WKC-003-6TAG-0001	2020-09-18	2. Submitted to AHJ
STTMP	Airport Elevated Guideway	660373-3BAG-003-6TAG-0001	2020-09-11	2. Submitted to AHJ
STTMP	Bronson and Heron South of Rail Tree Cutting (Mooney's)		2020-09-11	2. Submitted to AHJ
STTMP	Greenboro Station Tree Cutting		2020-01-20	2. Submitted to AHJ
STTMP	Carleton SSTTMP Temporary Works	660373-4CCM-003-6TAG-0002	2020-09-11	2. Submitted to AHJ
STTMP	Earl Armstrong Rail Bridge	660373-3BEA-003-6TAG-0001	2020-09-11	2. Submitted to AHJ
STTMP	Leitrim Rail Bridge SSTTMP	660373-3BLT-003-6TAG-0001	2020-09-11	2. Submitted to AHJ
STTMP	Lester Rail Bridge SSTTMP	660373-3BLR-003-6TAG-0001	2020-09-11	2. Submitted to AHJ
STTMP	SSTTMP Mooney's Bay Station	660373-1MBC-003-6TAG-0001	2020-09-11	2. Submitted to AHJ
STTMP	South Keys Station SSTTMP	660373-1SKC-003-6TAG-0001	2020-09-11	2. Submitted to AHJ
STTMP	Uplands Rail Bridge SSTTMP	660373-3BUD-003-6TAG-0001	2020-09-11	2. Submitted to AHJ
STTMP	Airport Station SSTTMP	660373-1APC-003-6TAG-0001	2020-09-11	2. Submitted to AHJ
STTMP	Uplands Station SSTTMP	660373-1ULC-003-6TAG-0001	2020-09-11	2. Submitted to AHJ
STTMP	Bronson and Heron East Tree Cutting (Mooney's)	660373-3BSC-003-6TAG-0001	2020-01-17	2. Submitted to AHJ
STTMP	Ellwood Diamond Grade Separation Structure over Transitway	660373-3BEW-003-6TAG-0001	2020-09-04	2. Submitted to AHJ
STTMP	Limebank Road Rail Bridge	660373-3BLB-003-6TAG-0001	2020-09-04	2. Submitted to AHJ
STTMP	Brookfield Pathway Tree Cutting (Transitway)		2020-06-01	2. Submitted to AHJ
STTMP	Bronson and Heron North of Rail Tree Cutting (Mooney's)		2020-02-21	2. Submitted to AHJ
STTMP	Bowesville Rail Bridge	660373-3BBW-003-6TAG-0001	2020-01-30	2. Submitted to AHJ
STTMP	Walkley West of Rail Tree Removal		2020-01-29	2. Submitted to AHJ
STTMP	Walkley West of Rail Tree Cutting		2020-01-17	2. Submitted to AHJ
STTMP	Brookfield South & Transitway Tree Cutting		2020-01-23	2. Submitted to AHJ
STTMP	Walkley East of Rail Tree Cutting		2020-01-14	2. Submitted to AHJ
STTMP	Carleton MUP Tree Cutting SSTTMP	660373-4CCM-003-6TAG-0001	2020-01-23	2. Submitted to AHJ
STTMP	Hintonburg Place & MUP Tree Cutting (Bayview)		2020-01-23	2. Submitted to AHJ
STTMP	SSTTMP for Bowesville Station	660373-1BWC-003-6TAG-0001	2020-09-11	3. Submitted Internally



# Trillium - SSTTMP Submissions

STTMP	Earl Armstrong & Bowesville Road Intersection Reconfiguration	660373-1CBL-003-6TAG-0001	2020-10-01	5. Overdue
STTMP	University Road Pedestrian Bridge (Carleton)	660373-3BUR-003-6TAG-0001	2020-10-01	5. Overdue
STTMP	Carling Station SSTTMP	660373-1CLC-003-6TAG-0002	2020-10-01	5. Overdue
STTMP	Leitrim Station and Park & Ride	660373-1LTC-003-6TAG-0001	2020-09-25	5. Overdue
STTMP	NCR Rail Crossing Relocation at Lester Road		2020-09-11	5. Overdue
STTMP	New Walkley MUP		2020-09-11	5. Overdue
STTMP	Osgoode Link Pathway Alternate Cycling Route		2020-09-11	5. Overdue
STTMP	Bayview	660373-1BVC-003-6TAG-0001	2020-03-26	5. Overdue

# Trillium - TCP Submissions

Type	Deliverable Title	Document No.	Anticipated Submission Date	Status
TCP	TCP Hunt Club EB Line Painting & Bus/Bike Lane Paint Removal	660373-3BHC-003-4TDK-TCP-0006	2020-10-27	1. Accepted
TCP	TCP Rideau River Coring	660373-4CCM-003-4TDK-TCP-0015	2020-10-26	1. Accepted
TCP	TCP Gladstone Station Trillium Access Road from the MUP	660373-4UBC-003-4TDK-TCP-0003	2020-10-21	1. Accepted
TCP	TCP Limebank & Earl Armstrong Accesses	660373-4CBL-003-4TDK-TCP-0014	2020-10-13	1. Accepted
TCP	TCP Leitrim Access from Gilligan Road	660373-4CSL-003-4TDK-TCP-0006	2020-10-13	1. Accepted
TCP	TCP Hunt Club Hydro Works South and South Caisson Pad Construction		2020-11-06	1. Accepted
TCP	TCP Carleton University - Station Works and Construction Access		2020-11-10	1. Accepted
TCP	TCP Greenboro Station - Construction Access from the Transitway			1. Accepted
TCP	TCP Bayview Construction Access			1. Accepted
TCP	TCP Road Closures for Span 8 Girder Install over Silver Star Private			1. Accepted
TCP	TCP Limebank SB Lanes Closure for Civil Works (5.0m Deep Sewer Connection)			1. Accepted
TCP	TCP Hunt Club Hydro Works South and South Caisson Pad Construction - Pedestrian Detour			1. Accepted
TCP	TCP Road Closures and Road Detour for Span 10 & 11 Girder Install over Paul Benoit Driveway and Silver Star Private			1. Accepted
TCP	TCP Hunt Club Road EB Lane Closure for Drill Rig Crossing			1. Accepted
TCP	TCP Advance Notice for Airport Pkwy NB On-Ramp Closures - for Drill Rig Crossing			1. Accepted
TCP	TCP Hunt Club TCB for Centre Caissons - Night Work	660373-3BHC-003-4TDK-TCP-0003	2020-09-24	1. Accepted
TCP	Earl Armstrong Girder Install TCP	660373-3BEA-003-4TDK-TCP-0001	2020-09-22	1. Accepted
TCP	TCP South Keys Access - Flagmen	660373-4CSU-003-4TDK-TCP-0026	2020-09-14	1. Accepted
TCP	TCP Airport Parkway SB Off-Ramp at Hunt Club Detour	660373-4CSU-003-4TDK-TCP-0022	2020-09-14	1. Accepted
TCP	TCP Airport Parkway SB Off-Ramp at Hunt Club Walkley Detour	660373-4CSU-003-4TDK-TCP-0023	2020-09-14	1. Accepted
TCP	TCP Hunt Club Hydro Works South - Pedestrian Detour	660373-4CSU-003-4TDK-TCP-0016	2020-09-14	1. Accepted
TCP	TCP Hunt Club Hydro Works South - Transitway Off-Ramp & Mac Street	660373-4CSU-003-4TDK-TCP-0018	2020-09-14	1. Accepted
TCP	TCP Hunt Club South Hydro Works South - Airport Parkway Off-Ramp	660373-4CSU-003-4TDK-TCP-0021	2020-09-14	1. Accepted
TCP	TCP Hunt Club Overall South Hydro Works	660373-4CSU-003-4TDK-TCP-0025	2020-09-11	1. Accepted
TCP	Leitrim Park & Ride Geotechnical Works BH3	660373-1LTC-003-4TDK-TPC-0003	2020-09-04	1. Accepted
TCP	TCP South Keys Station Access	660373-4CSU-003-4TDK-TCP-0024	2020-09-01	1. Accepted
TCP	Carling Station Tree Cutting TCP	660373-1CLC-003-4TDK-TCP-0001	2020-08-12	1. Accepted
TCP	TCP Transitway - Access Construction	660373-4CGS-003-4TDK-TCP-0002	2020-08-10	1. Accepted
TCP	TCP Earl Armstrong Access to East side of Mosquito Creek	660373-4CLB-003-4TDK-TCP-0009	2020-08-05	1. Accepted
TCP	TCP Hunt Club Central Caisson Works - TCB Installation & General Layout Overview	660373-3BHC-003-4TDK-TCP-0001	2020-07-30	1. Accepted
TCP	TCP Lester Deck Install - EB Closure	660373-3BLR-003-4TDK-TCP-0010	2020-07-09	1. Accepted
TCP	TCP Lester Deck Install - WB Closure	660373-3BLR-003-4TDK-TCP-0011	2020-07-09	1. Accepted

# Trillium - TCP Submissions

TCP	Transitway (Ellwood) Access Road	660373-4UMW-003-4TDK-TCP-0002	2020-06-30	1. Accepted
TCP	TCP Boreholes on the NB Airport Parkway Off-Ramp at Hunt Club Road	660373-4CSU-003-4TDK-TCP-0001	2020-06-29	1. Accepted
TCP	TCP South Keys Trillium Station MUP Temporary Detour - To be done first	660373-4UGS-003-4TDK-TCP-0002	2020-06-26	1. Accepted
TCP	TCP Bowesville As-built Road Girders Survey (East)	660373-3BBW-003-4TDK-TCP-0007	2020-06-17	1. Accepted
TCP	TCP Bowesville As-built Road Girders Survey (West)	660373-3BBW-003-4TDK-TCP-0008	2020-06-17	1. Accepted
TCP	TCP Uplands As-built Road Girders Survey (East)	660373-3BUD-003-4TDK-TCP-0005	2020-06-17	1. Accepted
TCP	TCP Uplands As-built Road Girders Survey (West)	660373-3BUD-003-4TDK-TCP-0006	2020-06-17	1. Accepted
TCP	Carling Avenue CCTV	660373-4CCC-003-4TDK-TCP-0003	2020-06-12	1. Accepted
TCP	Carling Avenue CCTV-2	660373-4CCC-003-4TDK-TCP-0004	2020-06-12	1. Accepted
TCP	Limebank Boreholes (NB Closure)	660373-4CBL-003-4TDK-TCP-0005	2020-06-11	1. Accepted
TCP	Limebank Boreholes (SB Closures)	660373-4CBL-003-4TDK-TCP-0006	2020-06-11	1. Accepted
TCP	Lester Girder Install Detour - Airprt & Hunt Clb	660373-3BLR-003-4TDK-TCP-0007	2020-06-05	1. Accepted
TCP	Lester Girder Install Detour - Overall	660373-3BLR-003-4TDK-TCP-0005	2020-06-05	1. Accepted
TCP	Bayview Hydrovac	660373-4UBG-003-4TDK-TCP-0003	2020-06-04	1. Accepted
TCP	Geotech TCP Carleton (North)	660373-4CCC-003-4TDK-TCP-0005	2020-06-03	1. Accepted
TCP	Rideau River Borehole TCP	660373-4CCM-003-4TDK-TCP-0010	2020-06-02	1. Accepted
TCP	Lester Girder Install Detour - Albion & Lester	660373-3BLR-003-4TDK-TCP-0009	2020-05-30	1. Accepted
TCP	Lester Girder Install Detour - Hunt Clb & Bank	660373-3BLR-003-4TDK-TCP-0008	2020-05-30	1. Accepted
TCP	Lester Girder Install Detour - Lester & Airport	660373-3BLR-003-4TDK-TCP-0006	2020-05-30	1. Accepted
TCP	Tree Cutting South of Carleton Station	660373-4CCM-003-4TDK-TCP-0009	2020-05-26	1. Accepted
TCP	Bowesville Rail Bridge Detour	660373-3BBW-003-4TDK-TCP-0003	2020-05-22	1. Accepted
TCP	Bowesville Rail Bridge Detour - Earl Armstrong	660373-3BBW-003-4TDK-TCP-0004	2020-05-22	1. Accepted
TCP	Bowesville Rail Bridge Detour - Rideau	660373-3BBW-003-4TDK-TCP-0005	2020-05-22	1. Accepted
TCP	Bowesville Rail Bridge Detour - Rideau/Albion	660373-3BBW-003-4TDK-TCP-0006	2020-05-22	1. Accepted
TCP	Geotech TCP Carleton (South)	660373-4CCM-003-4TDK-TCP-0008	2020-05-22	1. Accepted
TCP	Uplands Rail Bridge Detour	660373-3BUD-003-4TDK-TCP-0001	2020-05-20	1. Accepted
TCP	Uplands Rail Bridge Detour	660373-3BUD-003-4TDK-TCP-0002	2020-05-20	1. Accepted
TCP	Uplands Rail Bridge Detour	660373-3BUD-003-4TDK-TCP-0003	2020-05-20	1. Accepted
TCP	Uplands Rail Bridge Detour	660373-3BUD-003-4TDK-TCP-0004	2020-05-20	1. Accepted
TCP	Gladstone CCTV TCP EB Lane	660373-4CGC-003-4TDK-TCP-0006	2020-05-15	1. Accepted
TCP	Gladstone CCTV TCP WB Lane	660373-4CGC-003-4TDK-TCP-0007	2020-05-15	1. Accepted
TCP	Hintonburg Place CCTV - North (Bayview)	660373-4UBG-003-4TDK-TCP-0001	2020-05-15	1. Accepted
TCP	Hintonburg Place CCTV - South (Bayview)	660373-4UBG-003-4TDK-TCP-0002	2020-05-15	1. Accepted
TCP	Preston Street Area CCTV TCP (Carling)	660373-4CGC-003-4TDK-TCP-0001	2020-05-11	1. Accepted
TCP	Railway Street CCTV TCP (Carling)	660373-4CGC-003-4TDK-TCP-0004	2020-05-11	1. Accepted



# Trillium - TCP Submissions

TCP	Beech West or Railway CCTV TCP (Carling)	660373-4CGC-003-4TDK-TCP-0002	2020-05-08	1. Accepted
TCP	Limebank (East Side) Duct Works TCP	660373-4CBL-003-4TDK-TCP-0004	2020-05-08	1. Accepted
TCP	Carleton TCP Boreholes	660373-1CRC-003-4TDK-TCP-0002	2020-05-07	1. Accepted
TCP	Gladstone MUP Temporary Detour	660373-4UBC-003-4TDK-TCP-0001	2020-04-30	1. Accepted
TCP	Airport Parkway Girder Install - Hunt Clu/Parkway Intersection	660373-3BAP-003-4TDK-TCP-0004	2020-04-28	1. Accepted
TCP	Airport Parkway Girder Install - Leste/Hunt Clu Intersection	660373-3BAP-003-4TDK-TCP-0003	2020-04-28	1. Accepted
TCP	Airport Parkway Girder Install - Upland/Hunt Clu Intersection	660373-3BAP-003-4TDK-TCP-0002	2020-04-28	1. Accepted
TCP	Airport Parkway Girder Install Detour - Overall	660373-3BAP-003-4TDK-TCP-0001	2020-04-28	1. Accepted
TCP	Beech and Railway CCTV TCP (Carling)	660373-4CGC-003-4TDK-TCP-0003	2020-04-28	1. Accepted
TCP	Champagne and Young CCTV TCP (Carling)	660373-4CGC-003-4TDK-TCP-0005	2020-04-28	1. Accepted
TCP	Lester Road Concrete Works WB Lane Closure TCP	660373-3BLR-003-4TDK-TCP-0003	2020-04-28	1. Accepted
TCP	Uplands Drive (East Shoulder) Hydro Works	660373-4CUA-003-4TDK-TCP-0002	2020-04-15	1. Accepted
TCP	Uplands Drive (West Shoulder Duct Bank Installaion)	660373-4CUA-003-4TDK-TCP-0001	2020-04-15	1. Accepted
TCP	Lester TCB Caisson Drilling	660373-3BLR-003-4TDK-TCP-0001	2020-04-14	1. Accepted
TCP	Lester TCP for TCB Installation	660373-3BLR-003-4TDK-TCP-0002	2020-04-14	1. Accepted
TCP	TCP CCTV Carleton MUPs	660373-4CCM-003-4TDK-TCP-0006	2020-04-06	1. Accepted
TCP	TCP CCTV Carleton Station	660373-1CLC-003-4TDK-TCP-0003	2020-04-06	1. Accepted
TCP	TCP Lester Shoulder Repair	660373-4CSL-003-4TDK-TCP-0001	2020-04-01	1. Accepted
TCP	Carleton TCP - Laydown and MUP Reroute	660373-4CCM-003-4TDK-TCP-0003	2020-03-30	1. Accepted
TCP	TCP for CCTV Works - Earl Armstrong WB LT Lane	660373-1LBC-003-4TDK-TCP-0001	2020-03-22	1. Accepted
TCP	TCP Standard Closures for CCTV Works	660373-0000-003-4TDK-TCP-0001	2020-03-22	1. Accepted
TCP	Carleton TCP - Borehole Investigation	660373-4CCM-003-4TDK-TCP-0002	2020-02-24	1. Accepted
TCP	Airport Elevated Guideway Work Areas (TCP #2)	660373-3BAG-003-4TDK-TCP-0002	2020-02-21	1. Accepted
TCP	Carling Station Tree Cutting TCP 0002	660373-1CLC-003-4TDK-TCP-0002	2020-02-21	1. Accepted
TCP	Airport Elevated Guideway Clear Skies Closure	660373-3BAG-003-4TDK-TCP-0001	2020-02-14	1. Accepted

# Trillium - TCP Submissions

TCP	Leitrim Park & Ride Geotechnical Works BH1	660373-1LTC-003-4TDK-TPC-0001	2020-02-11	1. Accepted
TCP	Leitrim Park & Ride Geotechnical Works BH2	660373-1LTC-003-4TDK-TPC-0002	2020-02-11	1. Accepted
TCP	Earl Armstrong Access	Earl Armstrong Access	2020-01-17	1. Accepted
TCP	Lester TCP for TCB Removal	Lester TCP for TCB Removal	2020-01-15	1. Accepted
TCP	Bowesville Station Access via Earl Armstrong	Bowesville Station Temporary Access TCP	2020-01-13	1. Accepted
TCP	Limebank TCB	Limebank TCB	2020-01-09	1. Accepted
TCP	Earl Armstrong TCP for TCB	Earl Armstrong TCP for TCB Plan	2019-12-11	1. Accepted
TCP	Earl Armstrong TCB Plan	Earl Armstrong - TCB Plan	2019-12-04	1. Accepted
TCP	Lester Road EB Lane Closure	Lester Road EB Lane Closure- TCP	2019-12-02	1. Accepted
TCP	Lester Road WB Lane Closure	Lester Road WB Lane Closure- TCP	2019-12-02	1. Accepted
TCP	Lester Hydro Work	Lester Hydro work	2019-11-20	1. Accepted
TCP	Lester Road Railway Works TCP		2019-11-14	1. Accepted
TCP	Leitrim Hydro Works	Leitrim Hydro work	2019-10-29	1. Accepted
TCP	Earl Armstrong Hydro Install	Earl Armstrong - TCP for Hydro Duct Install	2019-10-22	1. Accepted
TCP	Bowesville Temporary Access	Bowesville Road - Temporary Access TCP	2019-10-15	1. Accepted
TCP	Greenboro Station - Borehole Drilling	Greenboro Station - Borehole Drilling	2019-10-15	1. Accepted
TCP	Bowesville Bridge TCB Plan	660373-TCB Plans(REV5 Acc lane)-Bowesville stamped	2019-10-11	1. Accepted
TCP	Airport Parkway Temporary Lanes Const	Airport Parkway - SBLaneClosure; Airport Parkway - NBLaneClosure	2019-10-07	1. Accepted
TCP	Airport Parkway Cyclist Detour	Airport parkway cyclist detour	2019-10-04	1. Accepted
TCP	Carling Station MUP Investigation	Carling Station MUP Investigation - TCP	2019-10-04	1. Accepted
TCP	Mooney's Bay Station MUP Investigation	Mooney's Bay Station MUP Investigation - TCP	2019-10-04	1. Accepted
TCP	Limebank Access - Curb Cutting	Limebank Curb Cut - Lane Closure TCP	2019-10-02	1. Accepted
TCP	Limebank Access	Limebank access	2019-09-25	1. Accepted
TCP	Earl Armstrong & Osgoode	Earl Armstrong & Osgoode-Detour	2019-09-06	1. Accepted
TCP	Airport Pkwy - TCP - Jersey Barrier Installation		2019-08-06	1. Accepted
TCP	Airport Parkway TCB Plan		2019-07-07	1. Accepted
TCP	TCP Earl Armstrong - WB Lane Closure for Bridge Works			2. ReSubmitted to AHJ
TCP	TCP Earl Armstrong - EB Lane Closure for Bridge Works			2. ReSubmitted to AHJ
TCP	TCP Hunt Club Hydro Works North - Pedestrian Detour			2. ReSubmitted to AHJ
TCP	TCP Hunt Club Road WB Lanes Closures for Median Concrete Pours			2. ReSubmitted to AHJ
TCP	TCP Advance warnings for Airport Pkwy NB Off-Ramp Closures - for Piers Concrete Pours			2. ReSubmitted to AHJ
TCP	TCP Mooney's Bay MUP Detour	660373-4CMW-003-4TDK-TCP-0016	2020-08-31	2. ReSubmitted to AHJ

# Trillium - TCP Submissions

TCP	TCP Carling MUP Detour - Option 1	660373-4CCC-003-4TDK-TCP-0008	2020-08-28	2. ReSubmitted to AHJ
TCP	Leitrim Road Water Chamber Inspections - Lane Closure	660373-3BBW-003-4TDK-TCP-0011	2020-08-20	2. ReSubmitted to AHJ
TCP	TCP South Keys Station Trillium MUP Temporary Detour - Second	660373-4UGS-003-4TDK-TCP-0001	2021-01-04	2. ReSubmitted to AHJ
TCP	Carleton Temporary MUP Construction	660373-3MCN-003-4TDK-TCP-0002	2020-07-07	2. ReSubmitted to AHJ
TCP	Transitway (Ellwood) Access Road East Side	660373-4UMW-003-4TDK-TCP-0003	2020-07-02	2. ReSubmitted to AHJ
TCP	Transitway (Ellwood) Daylighting	660373-4UMW-003-4TDK-TCP-0001	2020-06-17	2. ReSubmitted to AHJ
TCP	Junction Avenue Hydrovac (Daylighting)	660373-4CMW-003-4TDK-TCP-0008	2020-06-09	2. ReSubmitted to AHJ
TCP	TCP Earl Armstrong Duct Bank Installation	660373-4CLB-003-4TDK-TCP-0001	2020-05-26	2. ReSubmitted to AHJ
TCP	TCP CCTV @ Greenboro (EB Closure on Transitw)	660373-4CWG-003-4TDK-TCP-0001	2020-05-11	2. ReSubmitted to AHJ
TCP	TCP CCTV @ Greenboro (WB Closure on Transitw)	660373-4CWG-003-4TDK-TCP-0002	2020-05-11	2. ReSubmitted to AHJ
TCP	Bayview Boreholes TCP	660373-1BVC-003-4TDK-TCP-0001	2020-05-07	2. ReSubmitted to AHJ
TCP	Carleton - University Drive CCTV	660373-4CCC-003-4TDK-TCP-0002	2020-05-07	2. ReSubmitted to AHJ
TCP	Mooney's - Heron Road Off-Ramp Temporary Access	660373-4CMW-003-4TDK-TCP-0005	2020-05-07	2. ReSubmitted to AHJ
TCP	Walkley Borehole	660373-4CMW-003-4TDK-TCP-0006	2020-05-06	2. ReSubmitted to AHJ
TCP	Bayview Boreholes TCP-2	660373-1BVC-003-4TDK-TCP-0002	2020-05-04	2. ReSubmitted to AHJ
TCP	Gladstone Access Roads	660373-4UBC-003-4TDK-TCP-0002	2020-04-30	2. ReSubmitted to AHJ
TCP	Walkley Road CCTV	660373-4CMW-003-4TDK-TCP-0004	2020-04-28	2. ReSubmitted to AHJ
TCP	TCP Leitrim Valve Replacement - WB Lane Shift	660373-3BBW-003-4TDK-TCP-0012	2020-10-13	2. Submitted to AHJ
TCP	TCP Brookfield MUP - Preparation Work for Building the Access Road	660373-4CMW-003-4TDK-TCP-0012	2020-09-17	2. Submitted to AHJ
TCP	TCP Hunt Club South Hydro Works - Transitway Closure	660373-4CSU-003-4TDK-TCP-0027	2020-09-14	2. Submitted to AHJ
TCP	Bowesville Rail Structure		2020-09-11	2. Submitted to AHJ
TCP	CCC_Airport Parkway - Shoulder Work TPP - CCC		2020-09-11	2. Submitted to AHJ
TCP	CCC_Airport Parkway - TCP - Redo BH East		2020-09-11	2. Submitted to AHJ
TCP	Earl Armstrong Rail Structure		2020-09-11	2. Submitted to AHJ
TCP	Hunt Club @ Airprt Parkway		2020-09-11	2. Submitted to AHJ
TCP	Leitrim Rail Structure		2020-09-11	2. Submitted to AHJ
TCP	Lester Rail Structure		2020-09-11	2. Submitted to AHJ
TCP	Uplands Rail Structure		2020-09-11	2. Submitted to AHJ
TCP	Walkley Road MUP Construction		2020-09-11	2. Submitted to AHJ
TCP	TCP Hunt Club TCBs for Centre Caisson Works	660373-3BHC-003-4TDK-TCP-0002	2020-09-01	2. Submitted to AHJ
TCP	TCP Lester Guideway Intersection Construction Vehicle Crossing	660373-4CSL-003-4TDK-TCP-0005	2020-08-24	2. Submitted to AHJ
TCP	TIA Airport Parkway Survey Works - SB Lane Closure	660373-4CSU-003-4TDK-TCP-0007	2020-07-21	2. Submitted to AHJ
TCP	TIA Airport Parkway Survey Works - NB Lane Closure	660373-4CSU-003-4TDK-TCP-0008	2020-07-21	2. Submitted to AHJ
TCP	Leitrim Station Temporary Parking Lot Layout	660373-4CLB-003-4TDK-TCP-0007	2020-07-17	2. Submitted to AHJ
TCP	TCP Carling MUP Detour	660373-4CCC-003-4TDK-TCP-0007	2020-07-10	2. Submitted to AHJ
TCP	Carleton TCP - Rail MUP Closure and Rerouting	660373-3MCN-003-4TDK-TCP-0001	2020-07-07	2. Submitted to AHJ



# Trillium - TCP Submissions

TCP	TCP Earl Armstrong Access 2, during Road Closure by Others	660373-4CLB-003-4TDK-TCP-0006	2020-07-02	2. Submitted to AHJ
TCP	Carleton TCP - Reroute Southern MUP along river	660373-3MCS-003-4TDK-TCP-0001	2020-05-26	2. Submitted to AHJ
TCP	Limebank Median Cut	660373-4CBL-003-4TDK-TCP-0001	2020-03-30	2. Submitted to AHJ
TCP	Airport Elevated Guideway - Proposed Pedestrian Detour TCP #3	660373-3BAG-003-4TDK-TCP-0003	2020-03-27	2. Submitted to AHJ
TCP	Bronson and Heron East Tree Cutting (Mooney's)	660373-3BSC-003-4TDK-TCP-0001	2020-03-20	2. Submitted to AHJ
TCP	Carleton TCP - Alternative: Use Existing Univeristy Drive as Access	660373-4CCC-003-4TDK-TCP-0001	2020-03-17	2. Submitted to AHJ
TCP	Carleton TCP - Parking and Parking Lot Access	660373-1CRC-003-4TDK-TCP-0001	2020-03-17	2. Submitted to AHJ
TCP	Airport Elevated Guideway - Flagged Pedestrian Access TCP #4	660373-3BAG-003-4TDK-TCP-0004	2020-03-05	2. Submitted to AHJ
TCP	Bronson and Heron South of Rail Tree Cutting (Mooney's)	Bronson and Heron South of Rail Tree Cutting TCP	2020-02-25	2. Submitted to AHJ
TCP	Airport Parkway & Trillium Rail Tree Cutting TCP	660373-4CMW-003-4TDK-TCP-0001	2020-02-24	2. Submitted to AHJ
TCP	Carleton MUP Tree Cutting TCP	660373-4CCM-003-4TDK-TCP-0001	2020-02-12	2. Submitted to AHJ
TCP	Bronson and Heron North of Rail Tree Cutting (Mooney's)	Bronson and Heron North of Rail Tree Cutting TCP	2020-01-30	2. Submitted to AHJ
TCP	Walkley West of Rail Tree Removal	Walkley West of Rail Tree Removal TCP	2020-01-29	2. Submitted to AHJ
TCP	Hintonburg Place & MUP Tree Cutting (Bayview)	Hintonburg Place Tree Cutting TCP	2020-01-28	2. Submitted to AHJ
TCP	Mooney's Bay Station Tree Cutting	Mooney's Bay Station Tree Cutting TCP	2020-01-23	2. Submitted to AHJ
TCP	Railway Street Borehole TCP	Railway Street Borehole TCP	2020-01-23	2. Submitted to AHJ
TCP	Greenboro Station Tree Cutting	Greenboro Station Tree Cutting TCP	2020-01-20	2. Submitted to AHJ
TCP	Brookfield South & Transitway Tree Cutting	Brookfield South Tree-Cutting TCP	2020-01-17	2. Submitted to AHJ
TCP	MUP Tree Cutting Standard	Standard TCP for MUP Tree Cutting	2020-01-17	2. Submitted to AHJ
TCP	Walkley West of Rail Tree Cutting	Walkley West of Rail Tree Cutting	2020-01-17	2. Submitted to AHJ
TCP	Walkley East of Rail Tree Cutting	Walkley Tree Cutting East of Rail - TCP	2020-01-14	2. Submitted to AHJ
TCP	Brookfield Pathway Tree Cutting (Transitway)	Brookfield Pathway Tree-Cutting TCP	2020-01-13	2. Submitted to AHJ
TCP	Limebank TCP for TCB	Limebank TCP for TCB	2019-12-20	2. Submitted to AHJ
TCP	Airport Parkway NB Lane Closure	Airport Parkway - NBLaneClosure	2019-11-29	2. Submitted to AHJ
TCP	Airport Parkway SB Lane Closure	Airport Parkway - SBLaneClosure	2019-11-29	2. Submitted to AHJ
TCP	Airport Parkway Acceleration Lanes	Trillium TCP - Airport Parkway Acceleration Lanes	2019-09-27	2. Submitted to AHJ
TCP	Earl Armstron & Limebank		2019-09-16	2. Submitted to AHJ
TCP	TCP Leitrim Chamber Access - EB Lane Closure	660373-3BBW-003-4TDK-TCP-0015	2020-10-26	3. Submitted Internally
TCP	TCP Leitrim Chamber Access - WB Lane Closure	660373-3BBW-003-4TDK-TCP-0016	2020-10-26	3. Submitted Internally
TCP	TCP Airport Silver Star Private Partial Lane Closure	660373-4CUA-003-4TDK-TCP-0004	2020-10-16	3. Submitted Internally
TCP	TCP Leitrim TCB for WB Lane Shift	660373-3BBW-003-4TDK-TCP-0014	2020-10-14	3. Submitted Internally
TCP	TCP Hunt Club - Access for North Caisson Works	660373-3BHC-003-4TDK-TCP-0005	2020-10-13	3. Submitted Internally
TCP	TCP Leitrim EB Shoulder Closure for Watermain Chlorination	660373-3BBW-003-4TDK-TCP-0013	2020-10-13	3. Submitted Internally
TCP	TCP Transitway Access for Mass Hauling/Concrete Pours at Ellwood Diamond	660373-3BEW-003-4TDK-TCP-0002	2020-10-13	3. Submitted Internally
TCP	TCP Greenboro - Consruction Access	660373-4CWG-003-4TDK-TCP-0005	2020-10-13	3. Submitted Internally
TCP	TCP Greenboro - MUP Detour	660373-4CWG-003-4TDK-TCP-0006	2020-10-13	3. Submitted Internally

# Trillium - TCP Submissions

TCP	TCP Bayview for Construction Access	660373-4CBG-003-4TDK-TCP-0001	2020-10-12	3. Submitted Internally
TCP	TCP Bayview for Pedestrian Detours	660373-4CBG-003-4TDK-TCP-0002	2020-10-12	3. Submitted Internally
TCP	TCP Bayview for Flagging Pedestrians on the North Pathway	660373-4CBG-003-4TDK-TCP-0003	2020-10-12	3. Submitted Internally
TCP	TCP Rideau River Pedestrian Bridge - Construction Access	660373-4CCM-003-4TDK-TCP-0013	2020-10-05	3. Submitted Internally
TCP	TCP Rideau River Pedestrian Bridge - MUP Crossing	660373-4CCM-003-4TDK-TCP-0014	2020-10-05	3. Submitted Internally
TCP	TCP Carling Station West Access	660373-4CCC-003-4TDK-TCP-0010	2020-10-05	3. Submitted Internally
TCP	TCP Hunt Club - North Caisson Works	660373-3BHC-003-4TDK-TCP-0004	2020-10-05	3. Submitted Internally
TCP	TCP Transitway Access for the Construction of the Ellwood Diamond Structure	660373-3BEW-003-4TDK-TCP-0001	2020-10-01	3. Submitted Internally
TCP	TCP Earl Armstrong WB Lane Shift and EB Passing Lane Closure	660373-4CBL-003-4TDK-TCP-0008	2020-09-25	3. Submitted Internally
TCP	TCP Hunt Club South Caisson Works - Construction Access from Bus Loop			3. Submitted Internally
TCP	TCP Bayview Pedestrian Detour			3. Submitted Internally
TCP	TCP Bayview - Flaggers North of the MUP			3. Submitted Internally
TCP	TCP Hunt Club South Caisson Works - Flagger for Construction Access from Bus Loop			3. Submitted Internally
TCP	TCP Carleton University - Additional Construction Site Accesses for Civil Works			3. Submitted Internally
TCP	TCP Lester WB Lane Closure for Turtle Fence Installation			3. Submitted Internally
TCP	TCP Lester EB Lane Closure for Turtle Fence Installation			3. Submitted Internally
TCP	TCP Carleton University - South Pedestrian Detour for Bridge Works			3. Submitted Internally
TCP	TCP Carleton University Drive - EB Lane Closure for Bridge Works			3. Submitted Internally
TCP	TCP Carleton University Drive - WB Lane Closure for Bridge Works			3. Submitted Internally
TCP	TCP Carleton - Campus Ave Lane Reduction for North MUP Structure Lift			3. Submitted Internally
TCP	TCP Earl Armstrong EB Slow Lane Closure	660373-4CBL-003-4TDK-TCP-0009	2020-09-25	3. Submitted Internally
TCP	TCP Pedestrian Detour for Earl Armstrong Rd (South)	660373-4CBL-003-4TDK-TCP-0010	2020-09-25	3. Submitted Internally
TCP	TCP Limebank NB Slow Lane Closure	660373-4CBL-003-4TDK-TCP-0011	2020-09-25	3. Submitted Internally
TCP	TCP Limebank NB and SB Passing Lane Closures	660373-4CBL-003-4TDK-TCP-0012	2020-09-25	3. Submitted Internally
TCP	TCP Limebank SB Slow Lane Closure	660373-4CBL-003-4TDK-TCP-0013	2020-09-25	3. Submitted Internally
TCP	TCP Hunt Club North Hydro Works - Inside Lane Closures	660373-4CSU-003-4TDK-TCP-0028	2020-09-25	3. Submitted Internally
TCP	TCP Hunt Club North Hydro Works - Airport Pkwy NB On-Ramp Closure	660373-4CSU-003-4TDK-TCP-0029	2020-09-25	3. Submitted Internally
TCP	TCP Brookfield MUP Detour Construction (Flagmen)	660373-4CMW-003-4TDK-TCP-0017	2020-09-17	3. Submitted Internally
TCP	TCP for reversing vehicles into site	660373-4CSU-003-4TDK-TCP-0014	2020-09-09	3. Submitted Internally
TCP	TCP Greenboro - Access	660373-4CWG-003-4TDK-TCP-0007	2020-08-13	3. Submitted Internally
TCP	TCP SE Transitway Closure at Trillium Overpass	660373-4CMW-003-4TDK-TCP-0013	2021-02-05	3. Submitted Internally
TCP	TCP Walkley - Construction Access	660373-4CMW-003-4TDK-TCP-0014	2020-08-10	3. Submitted Internally
TCP	TCP Walkley - MUP Detour	660373-4CMW-003-4TDK-TCP-0015	2020-08-10	3. Submitted Internally
TCP	TCP Transitway - Access	660373-4CGS-003-4TDK-TCP-0003	2020-08-05	3. Submitted Internally
TCP	TCP Walkley - Hydro vacing Works	660373-4CMW-003-4TDK-TCP-0011	2020-07-28	3. Submitted Internally
TCP	TCP Airport Station Pedestrian Detour (Convair)	660373-4CUA-003-4TDK-TCP-0003	2020-07-21	3. Submitted Internally
TCP	TCP Hunt Club MH inspection - Median	660373-4CSU-003-4TDK-TCP-0002	2020-07-14	3. Submitted Internally

# Trillium - TCP Submissions

TCP	TCP Hunt Club MH inspection - WB Sidewalk	660373-4CSU-003-4TDK-TCP-0003	2020-07-14	3. Submitted Internally
TCP	TCP Leitrim WM Chamber Replacement	660373-4CSL-003-4TDK-TCP-0004	2020-07-10	3. Submitted Internally
TCP	TCP Hunt Club MH inspection - EB Bus Lane	660373-4CSU-003-4TDK-TCP-0005	2020-07-08	3. Submitted Internally
TCP	TCP Lane Closure for the CU South MUP Works	660373-4CCM-003-4TDK-TCP-0012	2020-07-08	3. Submitted Internally
TCP	TCP Construction Access for the CU South MUP Works	660373-4CCM-003-4TDK-TCP-0011	2020-07-08	3. Submitted Internally
TCP	TCP Airport Elevated Guideway to OMCIA (Spans 1 to 4)	660373-3BAG-003-4TDK-TCP-0006	2020-07-03	3. Submitted Internally
TCP	TCP Hunt Club MH inspection - TBC	660373-4CSU-003-4TDK-TCP-0004	2020-07-02	3. Submitted Internally
TCP	TCP for Transitway - Access Geometry Plan	660373-4UMW-003-4TDK-TCP-0004	2020-06-30	3. Submitted Internally
TCP	Transitway Borehole TCP	Transitway BH TCP	2019-11-19	3. Submitted Internally
TCP	TCP Gladstone Ave to Beech St Tree Clearing	660373-4CGC-003-4TDK-TCP-0008	2020-10-21	3. Submitted to AHJ
TCP	TCP Hunt Club for North Hydro Works			3. Submitted to AHJ
TCP	TCP Hunt Club - Pedestrian Detour for South Caisson Works			3. Submitted to AHJ
TCP	TCP for Brookfield MUP Detour	660373-4CMW-003-4TDK-TCP-0010	2020-09-17	3. Submitted to AHJ
TCP	TCP Albion Road N & Kitchener Ave Construction Vehicle Restrictions	660373-2WYC-003-4TDK-TCP-0001	2020-08-24	3. Submitted to AHJ
TCP	TCP Detour for the Airport Parkway NB On-Ramp (East)	660373-4CSU-003-4TDK-TCP-0013	2020-08-19	3. Submitted to AHJ
TCP	TCP Hunt Club North Hydro Works - Transitway NB On-Ramp and SB Off-Ramp Closures	660373-4CSU-003-4TDK-TCP-0010	2020-08-19	3. Submitted to AHJ
TCP	TCP Hunt Club Hydro Works North - Pedestrian Detour	660373-4CSU-003-4TDK-TCP-0009	2020-08-19	3. Submitted to AHJ
TCP	TCP Carling MUP Detour - Option 2 - OBSOLETE	660373-4CCC-003-4TDK-TCP-0009	2020-08-18	3. Submitted to AHJ
TCP	TCP Earl Armstrong 50kph Speed Board	660373-4CLB-003-4TDK-TCP-0008	2020-07-22	3. Submitted to AHJ
TCP	TCP Uplands Deck Install - NB Closure	660373-3BUD-003-4TDK-TCP-0008	2020-07-10	3. Submitted to AHJ
TCP	TCP Uplands Deck Install - SB Closure	660373-3BUD-003-4TDK-TCP-0007	2020-07-10	3. Submitted to AHJ
TCP	TCP Bowesville Deck Works - Lane Closure	660373-3BBW-003-4TDK-TCP-0009	2020-07-02	3. Submitted to AHJ
TCP	TCP Bowesville Deck Works - Lane Closure 2	660373-3BBW-003-4TDK-TCP-0010	2020-07-02	3. Submitted to AHJ
TCP	TCP Earl Armstrong Access 1, during Road Closure by Others	660373-4CLB-003-4TDK-TCP-0005	2020-07-02	3. Submitted to AHJ
TCP	TCP Leitrim Rail Bridge Girder Install - Detour	660373-3BLT-003-4TDK-TCP-0005	2021-01-04	3. Submitted to AHJ
TCP	TCP Leitrim Rail Bridge Girder Install - Detour at Albion/Leitrim	660373-3BLT-003-4TDK-TCP-0008	2021-01-04	3. Submitted to AHJ
TCP	TCP Leitrim Rail Bridge Girder Install - Detour at Bowesville/Earl Armstrong	660373-3BLT-003-4TDK-TCP-0007	2021-01-04	3. Submitted to AHJ
TCP	TCP Leitrim Rail Bridge Girder Install - Detour at Leitrim/Bowesville	660373-3BLT-003-4TDK-TCP-0006	2021-01-04	3. Submitted to AHJ
TCP	TCP Access for Daylighting at Greenboro	660373-4CGS-003-4TDK-TCP-0001	2020-06-25	3. Submitted to AHJ
TCP	Airport MH Works-1	660373-4UUA-003-4TDK-TCP-0001	2020-06-11	3. Submitted to AHJ
TCP	Airport MH Works-2	660373-4UUA-003-4TDK-TCP-0002	2020-06-11	3. Submitted to AHJ
TCP	Hunt Club (South) Ramp Access Acc/Dec & Geometry	660373-4CSL-003-4TDK-TCP-0003	2020-06-09	3. Submitted to AHJ
TCP	Dow's Lake Pumping Station Geotech	660373-4CCC-003-4TDK-TCP-0006	2020-06-04	3. Submitted to AHJ

# Trillium - TCP Submissions

TCP	Greenboro Station daylighting	660373-4CWG-003-4TDK-TCP-0004	2020-05-28	3. Submitted to AHJ
TCP	Walkley Daylighting	660373-4CWG-003-4TDK-TCP-0003	2020-05-25	3. Submitted to AHJ
TCP	Junction Ave Geotech (Brookfield/Ellwood)	660373-4CMW-003-4TDK-TCP-0007	2020-05-21	3. Submitted to AHJ
TCP	Leitrim Equipment Crossing TCP	660373-3BLT-003-4TDK-TCP-0004	2020-05-15	3. Submitted to AHJ
TCP	Airport - Closure of Silver Star Private - Detour	660373-3BAG-003-4TDK-TCP-0005	2020-05-12	3. Submitted to AHJ
TCP	Leitrim Watermain Relocation	660373-3BLT-003-4TDK-TCP-0003	2020-05-12	3. Submitted to AHJ
TCP	Carleton Water HydroVac Daylighting TCP	660373-4CCM-003-4TDK-TCP-0007	2020-05-05	3. Submitted to AHJ
TCP	Lester Road Concrete Works EB Lane Closure TCP	660373-3BLR-003-4TDK-TCP-0004	2020-04-30	3. Submitted to AHJ
TCP	Earl Armstrong Hydro Works	660373-4CBL-003-4TDK-TCP-0003	2020-04-23	3. Submitted to AHJ
TCP	TCP CCTV Carleton Campus South-1	660373-4CCM-003-4TDK-TCP-0004	2020-04-06	3. Submitted to AHJ
TCP	TCP CCTV Carleton Campus South-2	660373-4CCM-003-4TDK-TCP-0005	2020-04-06	3. Submitted to AHJ
TCP	TCP CCTV Junction Avenue	660373-4CMW-003-4TDK-TCP-0003	2020-04-03	3. Submitted to AHJ
TCP	TCP CCTV Traverse Drive	660373-4CMW-003-4TDK-TCP-0002	2020-04-03	3. Submitted to AHJ
TCP	Limebank Median Temporary Cut Layout	660373-4CBL-003-4TDK-TCP-0002	2020-03-30	3. Submitted to AHJ
TCP	Leitrim Road Geometry Plan Access	660373-3BLT-003-4TDK-TCP-0002	2020-03-19	3. Submitted to AHJ
TCP	TCP Hunt Club MH inspection - EB Left Lane Closure	660373-4CSU-003-4TDK-TCP-0006		4. In Progress
TCP	South Keys Access Route (with MUP flagger)	660373-4CSL-003-4TDK-TCP-0002		
TCP	Airport Guideway - Girder Install Over Clear Skies	660373-3BAG-003-4TDK-TCP-000X		
TCP	Bayview TCP Temporary MUP Reroute	660373-4CGB-003-4TDK-TCP-0001		
TCP	Bowesville Construction Vehicle Crossing	660373-4CBL-003-4TDK-TCP-0007		
TCP	Bronson and Heron (East) Tree Cutting (Mooney's) - West of Transitwa	660373-3BSC-003-4TDK-TCP-0002		
TCP	Earl Armstrong Construction Vehicle Crossing	660373-4CLB-003-4TDK-TCP-0004		
TCP	Earl Armstrong South TCB for Duct Works	660373-4CLB-003-4TDK-TCP-0002		
TCP	Earl Armstrong South TCP for EB Lane Closure for Small Area of Work - Duct Banks	660373-4CLB-003-4TDK-TCP-0003		
TCP	Junction Avenue Geotech Works along Tracks	660373-4CMW-003-4TDK-TCP-0009		
TCP	Leitrim Water Works - EB Lane Closure	660373-3BBW-003-4TDK-TCP-0001		
TCP	Leitrim Water Works - WB Lane Closure	660373-3BBW-003-4TDK-TCP-0002		
TCP	Limebank Rail Structure			
TCP	TCP Limebank SB Slow Lane Closure for Civil Works - Stage 1 - OBSOLETE			7. Cancelled
TCP	TCP Limebank SB Slow Lane Closure for Civil Works - Stage 2 - OBSOLETE			7. Cancelled
TCP	TCP Bayview Station - MUP Detour for MUP Final Layout Construction			



# Trillium - TCP Submissions

TCP	TCP Brookfield - Additional Laydown Area		
TCP	TCP EY Centre Parking Closure for Uplands Station Civil Works		
TCP	TCP at Greenboro for the installation of CIH No. 5		
TCP	TCP AEG Tracker NB closed - Precast panel, formwork and concrete placement		
TCP	TCP AEG Tracker SB closed - Precast panel, formwork and concrete placement		
TCP	TCP AEG Silver star NB closed - Precast panel, formwork and concrete placement		
TCP	TCP AEG Silver star SB closed - Precast panel, formwork and concrete placement		
TCP	TCP AEG Paul Benoit EB closed - Precast panel, formwork and concrete placement		
TCP	TCP AEG Paul Benoit WB closed - Precast panel, formwork and concrete placement		
TCP	TCP Hunt Club Road EB Lane Closure for Concrete Placement in the Median (Contingency Dates)		
TCP	TCP Advance Notice for Ramp Closures at Hunt Club Road - for Concrete Placement in the Median (Contingency Dates)		
TCP	TCP Hunt Club Road EB Lane Closure for Drill Rig Crossing (Back to North Work Area)		
TCP	TCP Advance Notice for Ramp Closures at Hunt Club Road - for Drill Rig Crossing (Back to North Work Area)		
TCP	TCP Leitrim Bridge Superstructure Works; precast panel, formwork and concrete placement (WB)		
TCP	TCP Leitrim Bridge Superstructure Works; precast panel, formwork and concrete placement (EB)		
TCP	TCP Rideau River Eastern Pathway Closure		
TCP	TCP Rideau River Eastern Pathway Flagging for Bridge Construction		
TCP	TCP and Locates for Albion Road Ditch - OSPS		
TCP	TCP Rideau River - Daylighting Rogers and Bell 360 at South Approach		
TCP	TCP Leitrim Park & Ride South Side of Access Road Installation of Culvert for Dewatering Hose		
TCP	TCP Leitrim Park & Ride North Side of Access Road Installation of Culvert for Dewatering Hose		
TCP	TCP - Transitway Ellwood Temporary Early Speed Reduction to 50km/h		
TCP	TCP Leitrim Park & Ride Sidewalk Installation of Culvert for Dewatering Hose		
TCP	TCP Transitway NB Lane Reduction for Storm Sewer Investigation		
TCP	TCP Hunt Club for Centre Caisson Works - Settlement Monitoring		
TCP	TCP Bayview Use of Temporary MUP		
TCP	TCP Paul Benoit Tim Hortons Access Closure		