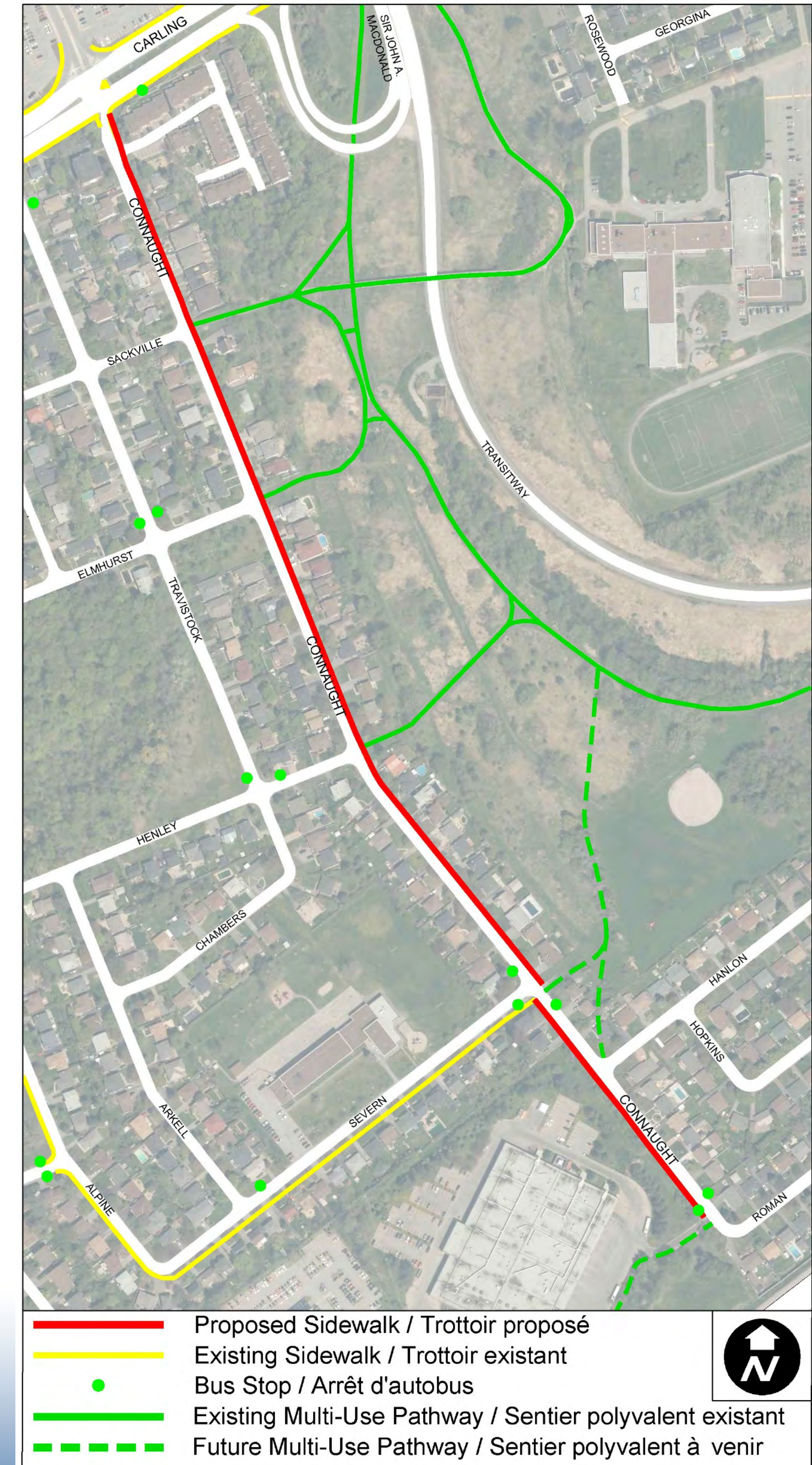


Connaught Avenue Proposed Pedestrian Facility Project

(from Carling Avenue to Roman Avenue)



2013 Ottawa Pedestrian Plan & Policy

- Connaught Avenue is a Phase 2 *Ottawa Pedestrian Plan* (OPP) project. The OPP can be viewed on the City of Ottawa website at:
 - <https://ottawa.ca/en/planning-development-and-construction/official-plan-and-master-plans#ottawa-pedestrian-plan>
- Policy in the OPP states that local roads that lead directly to a public park (e.g. Elmhurst Park and Connaught Park) and transit (e.g. future Queensview Station) should have a pedestrian facility.
- A sidewalk along Connaught Avenue aligns with the City's transportation goals and objectives to maximize walkability and create safe pedestrian environments for vulnerable users such as children, seniors, wheelchair users and persons with a vision disability.
- The project will provide greater access to the trails and multi-use pathways that connect at Sackville Street, Elmhurst Street and Henley Street. A new sidewalk will also connect to the future multi-use pathway proposed (through Stage 2 LRT project) at Severn Avenue.

Proposed Pedestrian Facilities

- Two options are proposed for new pedestrian facilities along Connaught Avenue, from Carling Avenue to Roman Avenue:
 - Option 1 – Sidewalk along the full length of the street
 - Option 2 – Sidewalk along only the north and south portions of the street
- In both options, additional pedestrian measures are proposed to create a more comfortable pedestrian environment.

Construction Timing

- City's estimated timeline for this proposed project:

2021 Finalize Approvals

2021 Detailed Design

2022 Construction – Phase 1

- Carling Avenue to Henley Street*

* Southern limit of construction will depend on the selected option and coordination with Stage 2 LRT construction

2026 Construction – Phase 2

- South limit of Phase 1 to Roman Avenue

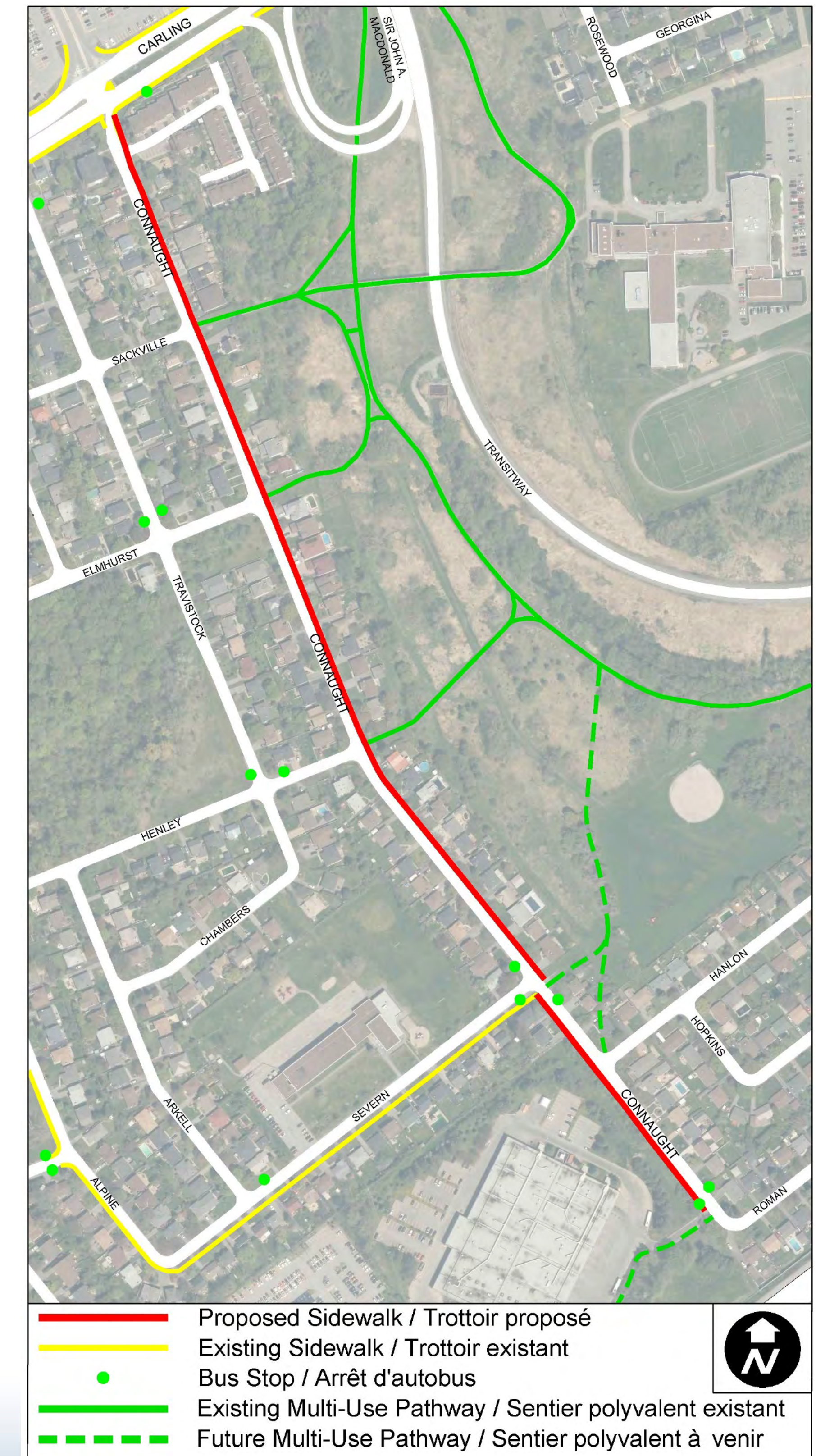
Option 1

ADVANTAGES:

- ❑ Provides a sidewalk the full length of Connaught Avenue connecting Carling Avenue, the future Queensview LRT Station, community parks and linkages to the Pinecrest Creek Pathway.
- ❑ No removal of mature trees anticipated ahead of construction

DETAILS:

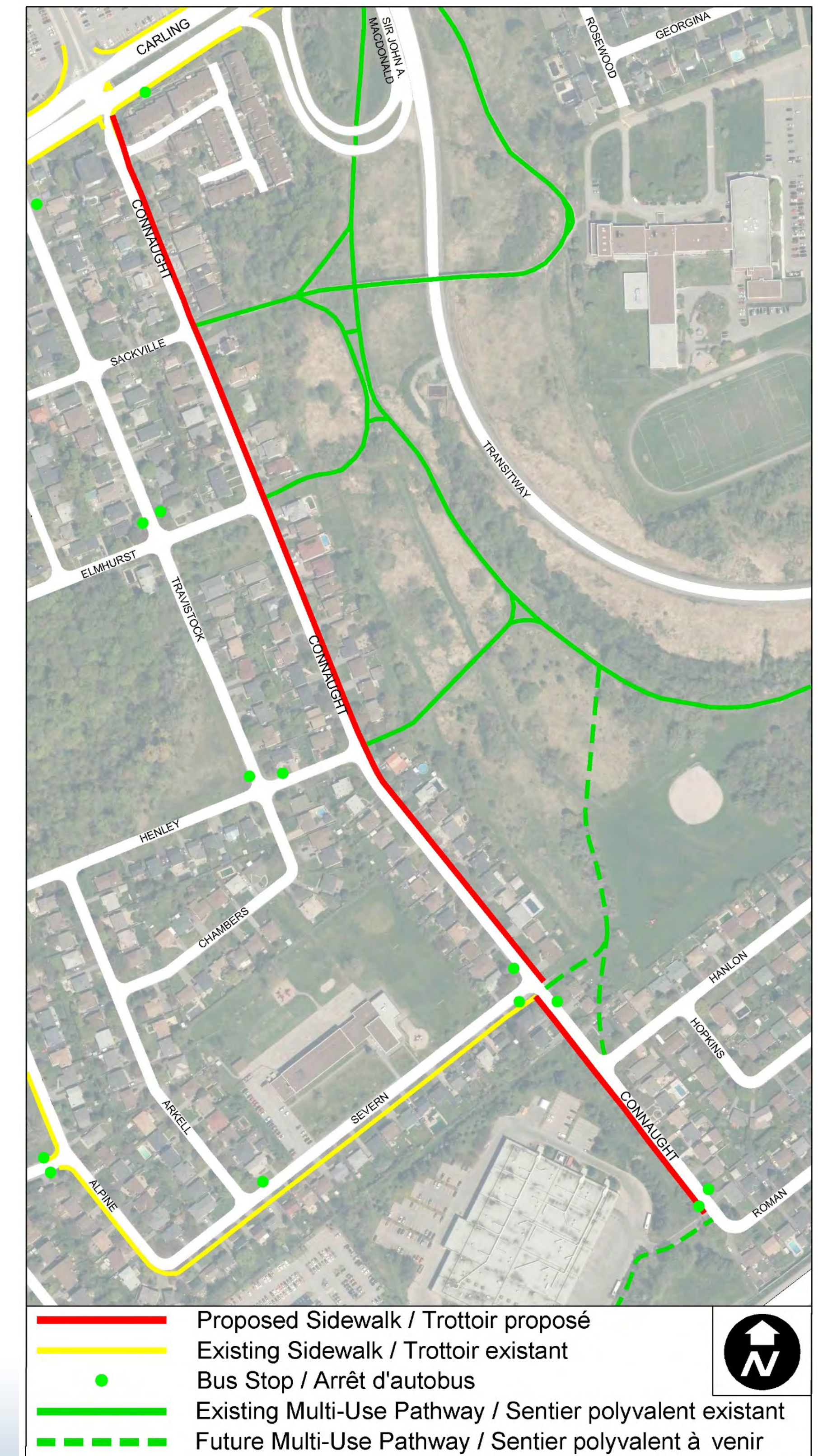
- Sidewalk along the east side of Connaught Avenue, from Carling Avenue to Severn Avenue
 - 2.0 m wide; 740 m in length
 - Roadway width of 7.0 m
- Sidewalk along the west side of Connaught Avenue, from Severn Avenue to Roman Avenue
 - West-side sidewalk would connect directly to the future multi-use pathway leading to Queensview LRT Station at the south end of Connaught Avenue and have less impact on property owners
 - 2.0 m wide; 220 m in length
 - Roadway width is unchanged
- Potential risk to mature street trees
 - City Forestry reviewed all trees along the east side of Connaught Ave. and eight mature trees were identified as “At Risk.” City Forestry anticipates all “at risk” trees can be retained due to large front yards (to support root growth) and the implementation of appropriate tree protection measures (e.g. construction/pruning techniques, reduced sidewalk width in front of “at risk” tree)
 - Sidewalk width reduced to 1.5 m in front of “at risk” trees



Option 1

DETAILS CONTINUED:

- Additional pedestrian features and traffic calming measures are proposed at intersections (Sackville Street, Henley Street and Severn Avenue).
- Tactile walking surface indicators (TWSI) installed at pedestrian crossings along the new sidewalk.
- All street trees would be protected during construction with proper root pruning and specialized construction techniques.
- On-street parking would be restricted to one side of the street where the road width is narrowed to 7.0 m (required for emergency vehicle access).
 - In locations where there is a gravel shoulder opposite the new sidewalk, on-street parking is recommended to be on the non-sidewalk side of the street
 - In locations where there is an existing curb opposite the new sidewalk, on-street parking could be on either side of the street, but not both.



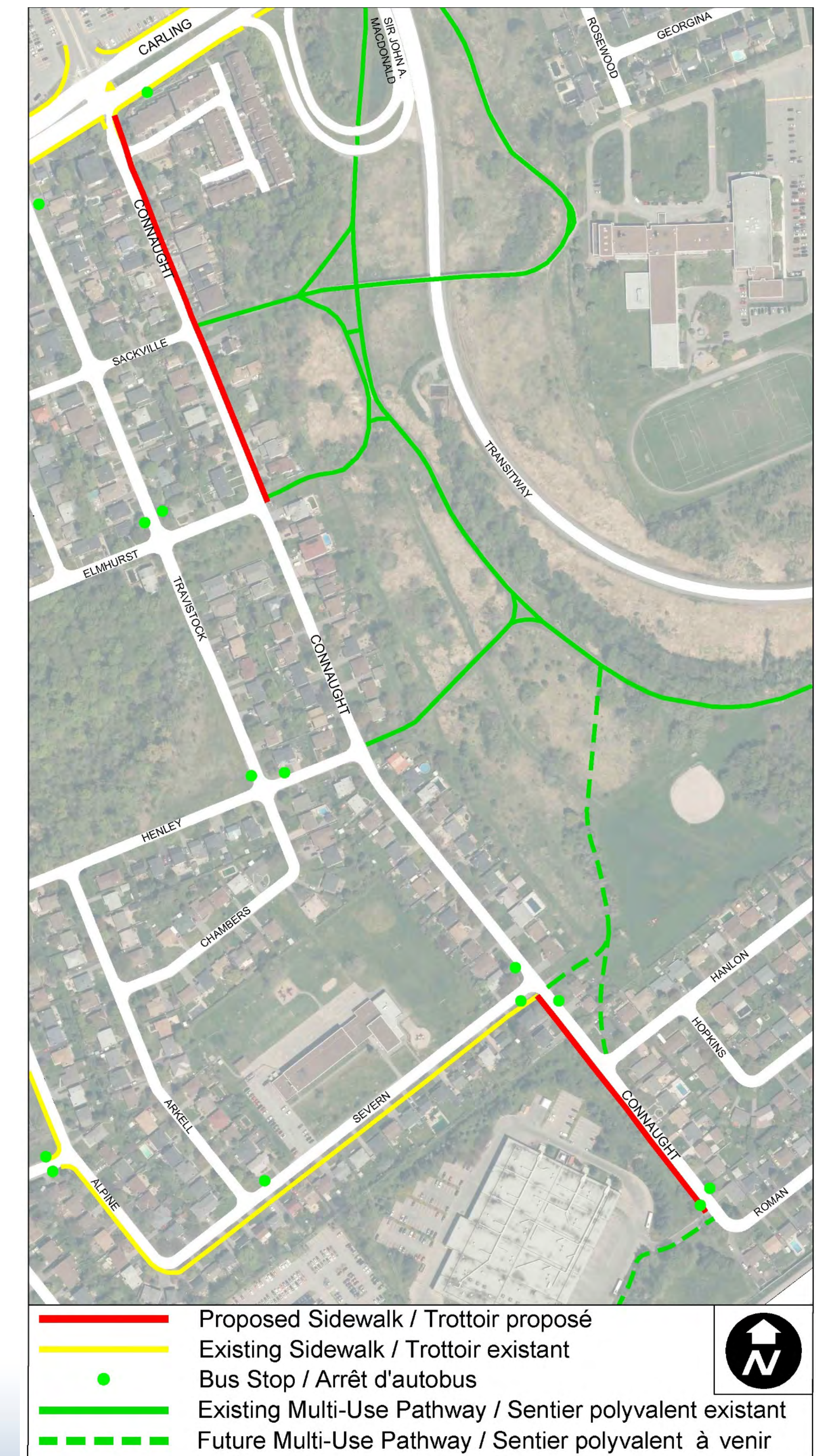
Option 2

ADVANTAGES:

- ❑ Low risk to street tree health
- ❑ Existing on-street parking remains unchanged along most of the street

DETAILS:

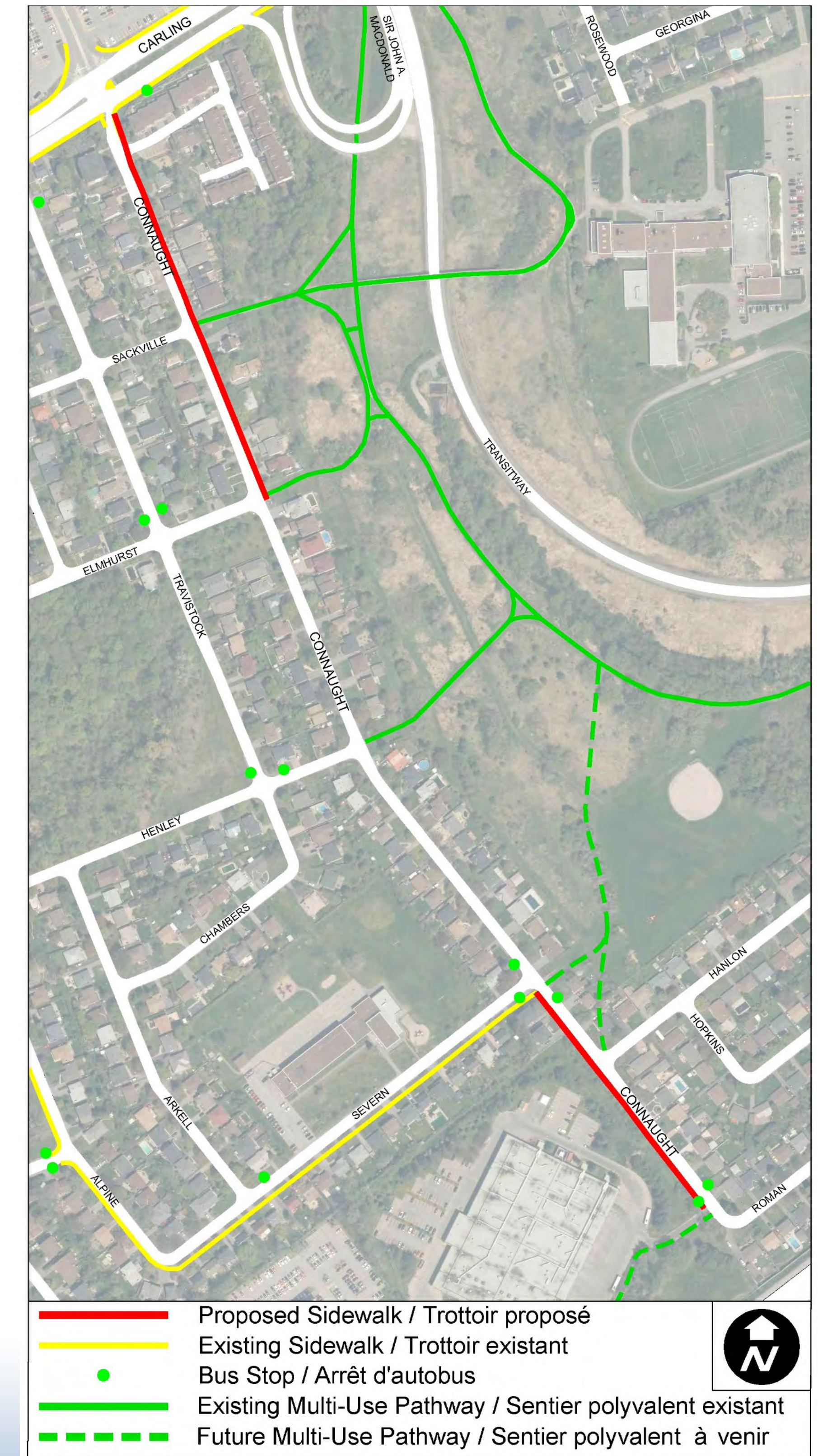
- Sidewalk along the east side of Connaught Avenue, from Carling Avenue to Elmhurst Street
 - 2.0 m wide; 315 m in length
 - Roadway width of 7.0 m
- Sidewalk along the west side of Connaught Avenue, from Severn Avenue to Roman Avenue
 - A west-side sidewalk would connect directly to the future MUP leading to Queensview LRT Station at the south end of Connaught Avenue and have less impact on property owners
 - 2.0 m width; 220 m in length
 - Roadway width is unchanged
- No “At Risk” trees identified by City Forestry



Option 2

DETAILS CONTINUED:

- Additional pedestrian features and traffic calming measures are proposed at intersections (Sackville Street, Henley Street and Severn Avenue).
- Tactile walking surface indicators (TWSI) installed at pedestrian crossings along the new sidewalk.
- On-street parking would be restricted to one side of the street where the road width is 7.0 m (required for emergency vehicle access).
 - In locations where there is a gravel shoulder opposite the new sidewalk, on-street parking is recommended to be on the non-sidewalk side of the street



Additional Pedestrian Measures

- Connaught Avenue / Sackville Street intersection:
 - New raised pedestrian crossover (PXO) on the north side of the intersection
 - Will connect to the existing multi-use pathway on the east side of Connaught Avenue
- Connaught Avenue / Henley Street intersection:
 - Existing crosswalk on the north side of the intersection is proposed to be raised
 - Will connect the existing multi-use pathway on the east side of Connaught Avenue to a future sidewalk on the north side of Henley Street
- Connaught Avenue / Severn Avenue intersection:
 - New at-grade pedestrian crossover (PXO) on the south side of the intersection
 - Facility proposed to be at-grade to not impact OC Transpo operations on Connaught Avenue
 - Will connect the future multi-use pathway on the east side of Connaught Avenue (built through Stage 2 LRT project) to the existing sidewalk on the south side of Severn Avenue

Tactile Walking Surface Indicator (TWSI)

TWSIs are plates installed level with the concrete that are meant to be detectable underfoot or with a cane. They are used to warn people of a potential hazard such as a roadway.



At-Grade Pedestrian Crossover (PXO)

Pedestrian Crossovers are designated areas that allow pedestrians to safely cross a road where vehicles must yield to pedestrians when crossing.

PROS

- Improves safety of pedestrians crossing a street
- Increases driver awareness of pedestrians
- Allows pedestrians (and cyclists once dismounted) to have priority and cross the street without delay

CONS

- Cyclists must dismount and walk.



Raised Pedestrian Crossover (PXO) / Raised Crosswalk

Raised crossings are marked pedestrian crossings at intersections, or mid-block locations, constructed at a higher elevation than the adjacent roadway.

PROS

- Reduces vehicle speeds at / near the crossing
- Self-enforcing
- Reinforces the stop condition, if present
- Improves proportion of drivers yielding to vulnerable road users
- Increased comfort for vulnerable road users


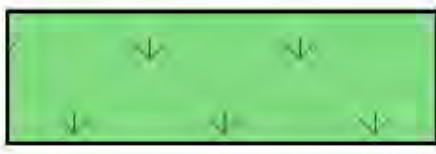
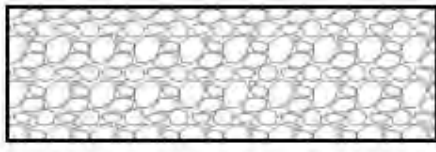




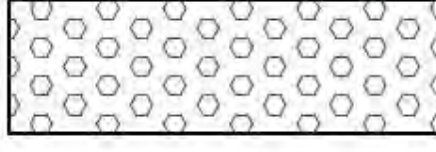





CONS

- May increase traffic induced noise and vibrations
- May impact emergency response activities
- May cause discomfort to transit users* and drivers
 - * On Connaught, transit will **not** operate across the raised crossing
- May increase vehicle travel time
- May result in a false sense of security

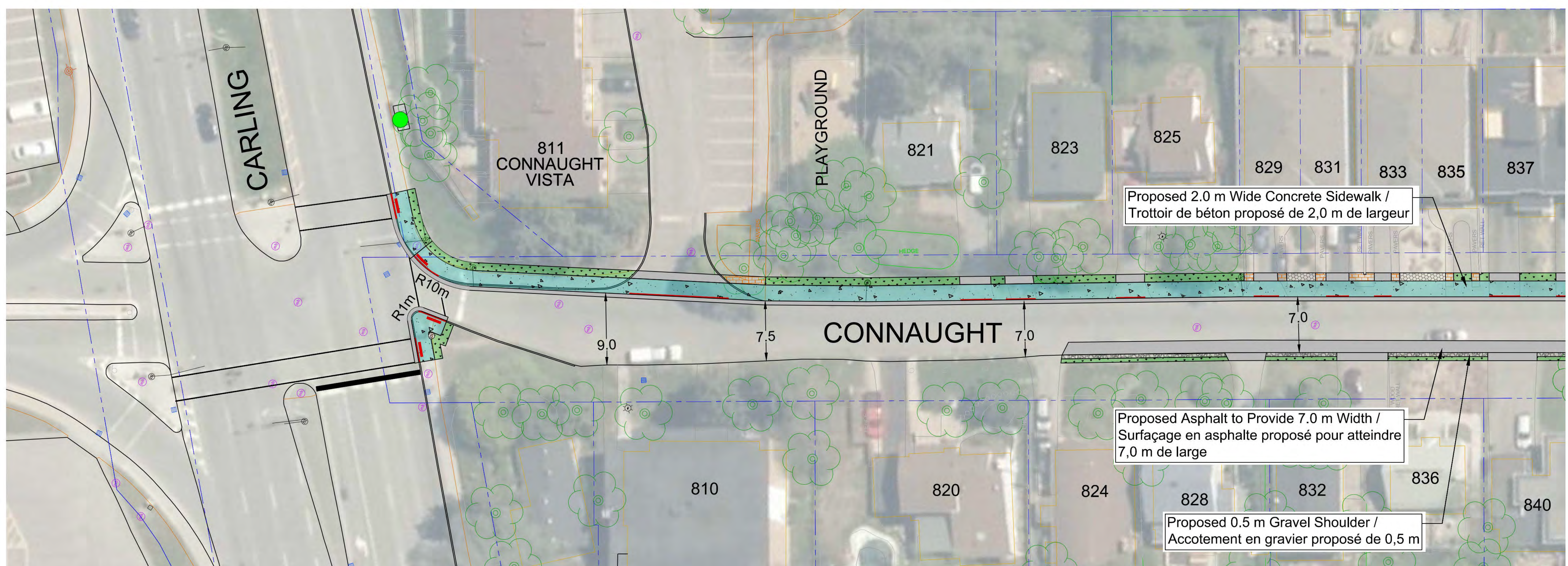


Design Options

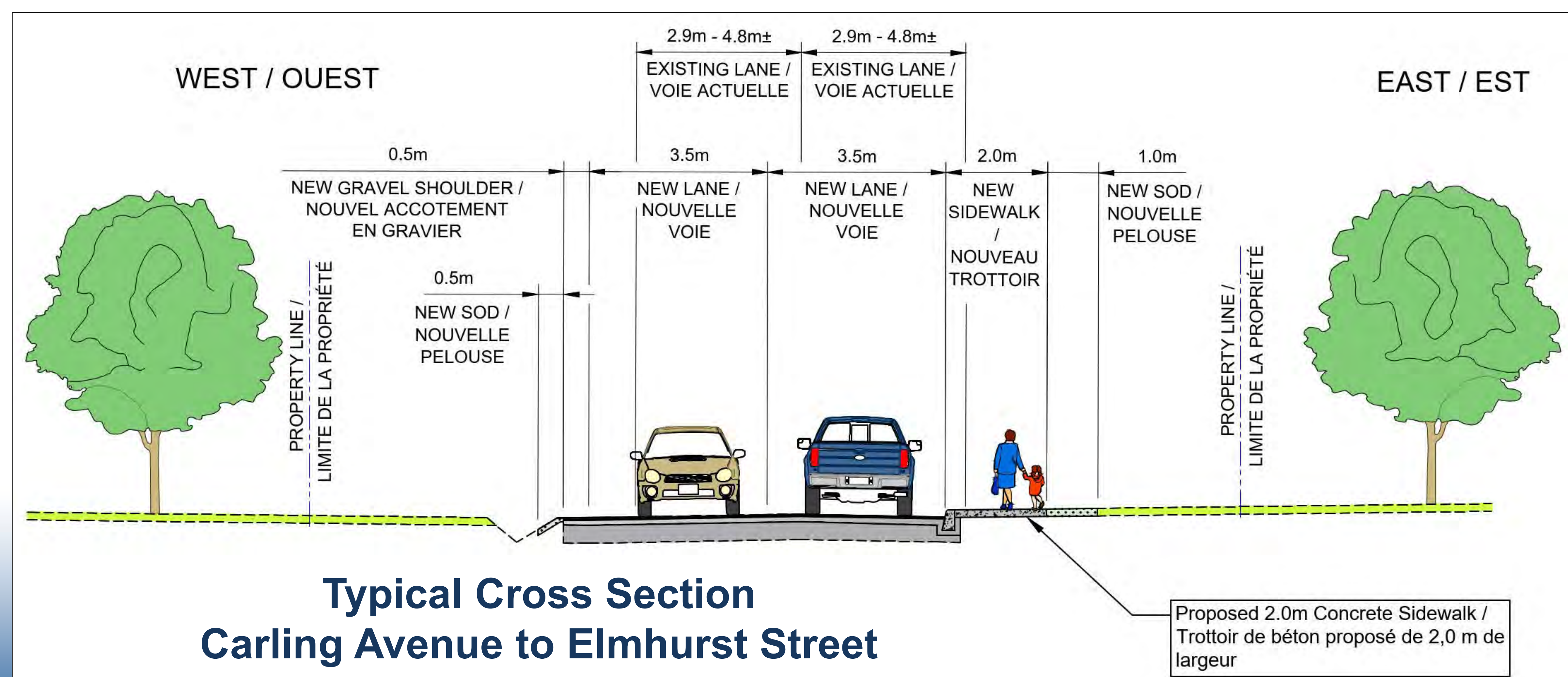
- Details of both design options are illustrated in the following slides from Carling Avenue to Roman Avenue.
- Where design details are the same for both options, only one illustration is shown.
- The following legend is the same for all drawings.

Legend / Légende	
	New Concrete Paving Nouveau pavage en béton
	Sod Gazon
	Gravel Shoulder Accotement en gravier
	Property line Limite de propriété
	Bus Stop Arrêt d'autobus
	New Asphalt Paving Nouveau pavage en asphalte
	Paver Driveway Entrée de cour pavée
	Stone Ground Cover Lit de pierres
	New Concrete Curb Nouvelle bordure en béton
	Depressed Curb Bordure surbaissée
	Tactile Walking Surface Indicator (TWSI) Indicateur tactile de surface de marche
	New 2 Rail Post and Rail Fence Nouvelle clôture de perche à deux lisses
	Tree Identified as at Risk Arbres identifiés « à risque »

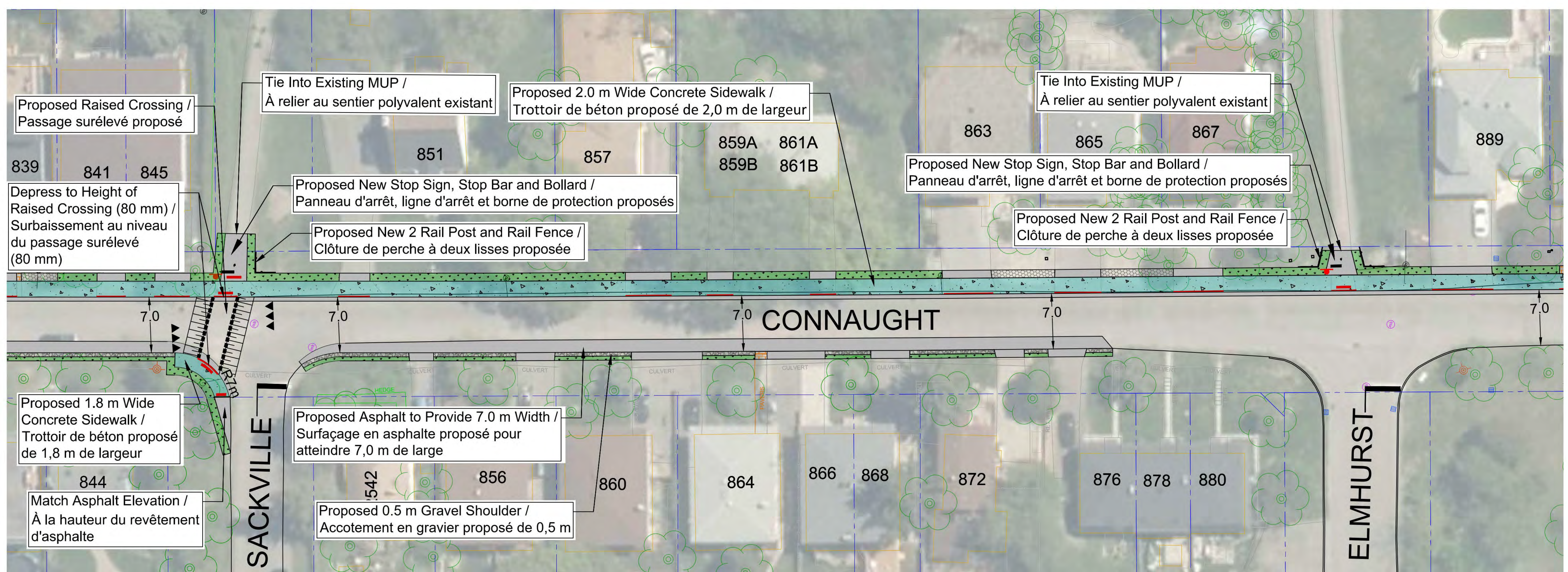
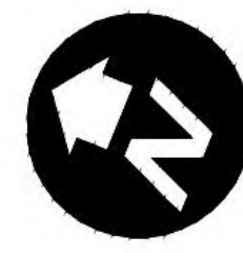
General Notes:
• All Grading to Provide Positive Drainage to the Road.
• Strips of Residential Driveways to be Removed and Reinstated.
• Width of Removal and Reinstatement of Residential Driveways Will Vary Depending on Slope Required for Drainage.
• Protect Existing Trees Within Work Zone.
Design Notes:
• Sidewalk Width to be Reduced to 1.5 m in Front of Trees Identified as at Risk by City Forestry.



Options 1 and 2: Carling Avenue to north of Sackville Street

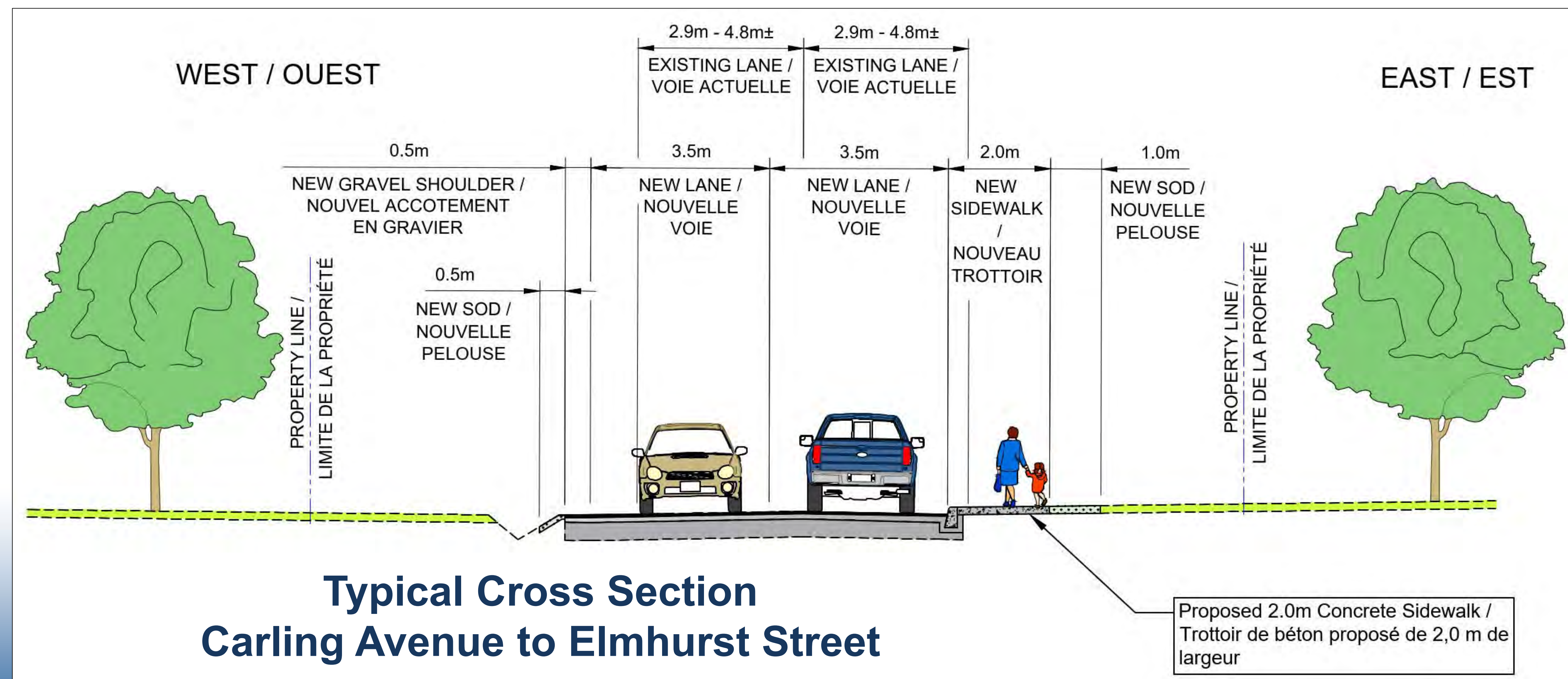


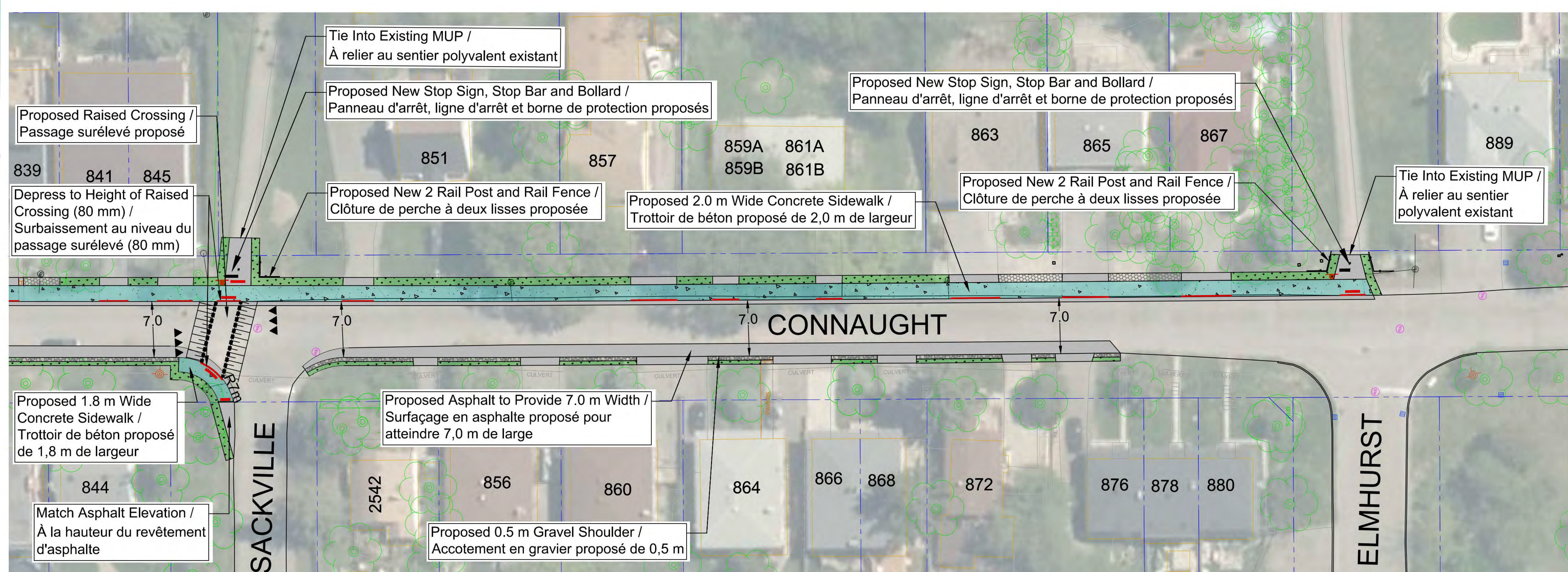
**Typical Cross Section
Carling Avenue to Elmhurst Street**



Option 1:

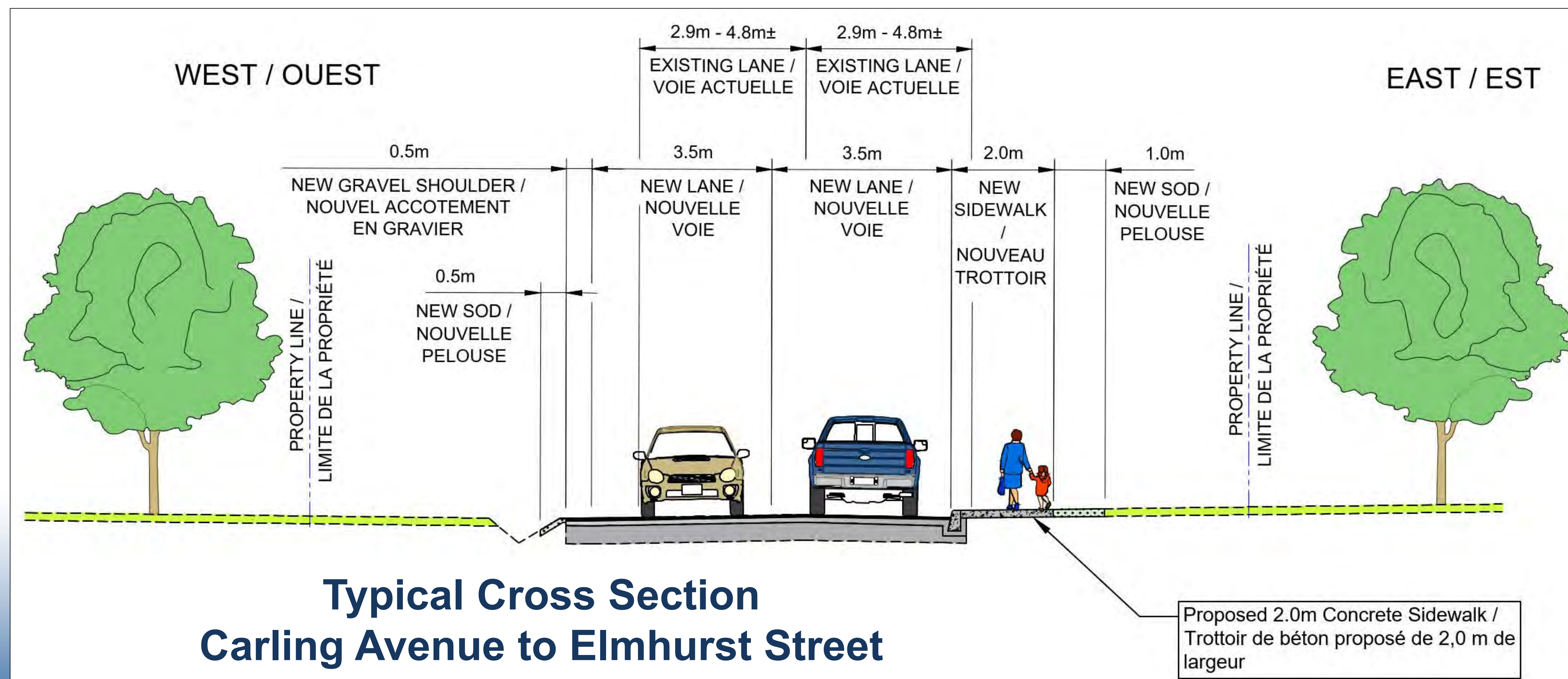
North of Sackville Street to south of Elmhurst Street

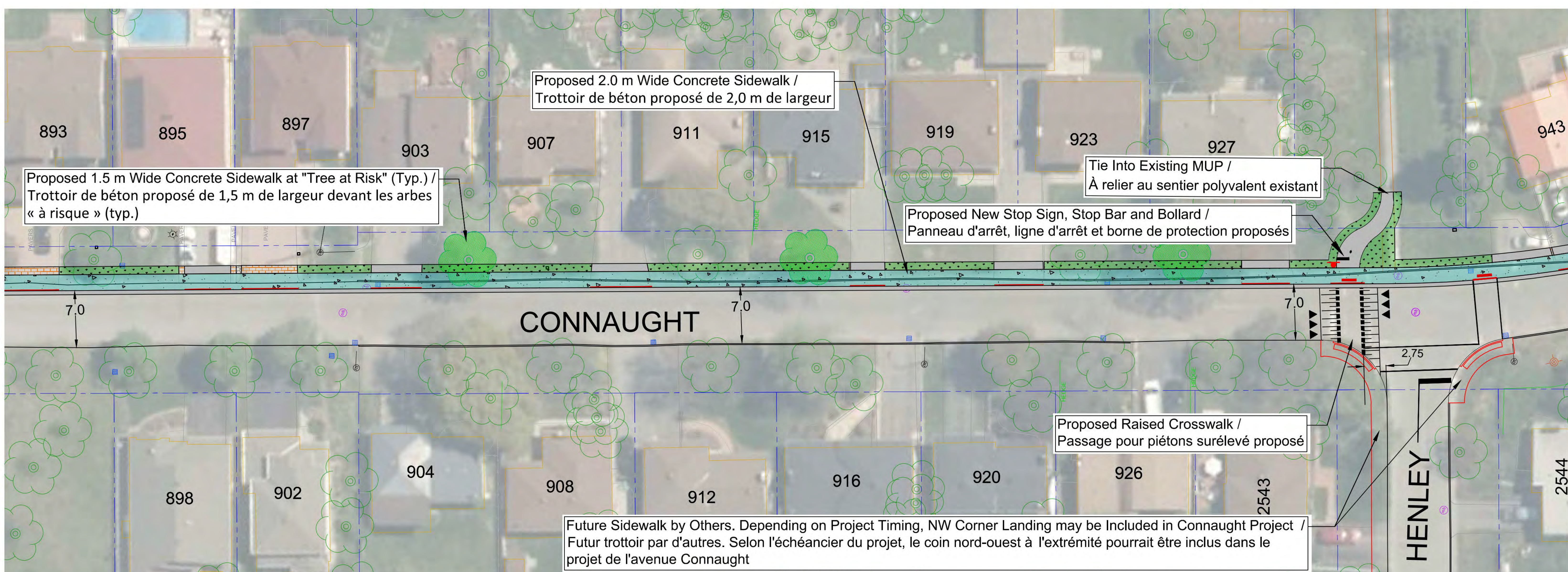
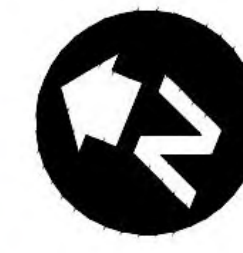




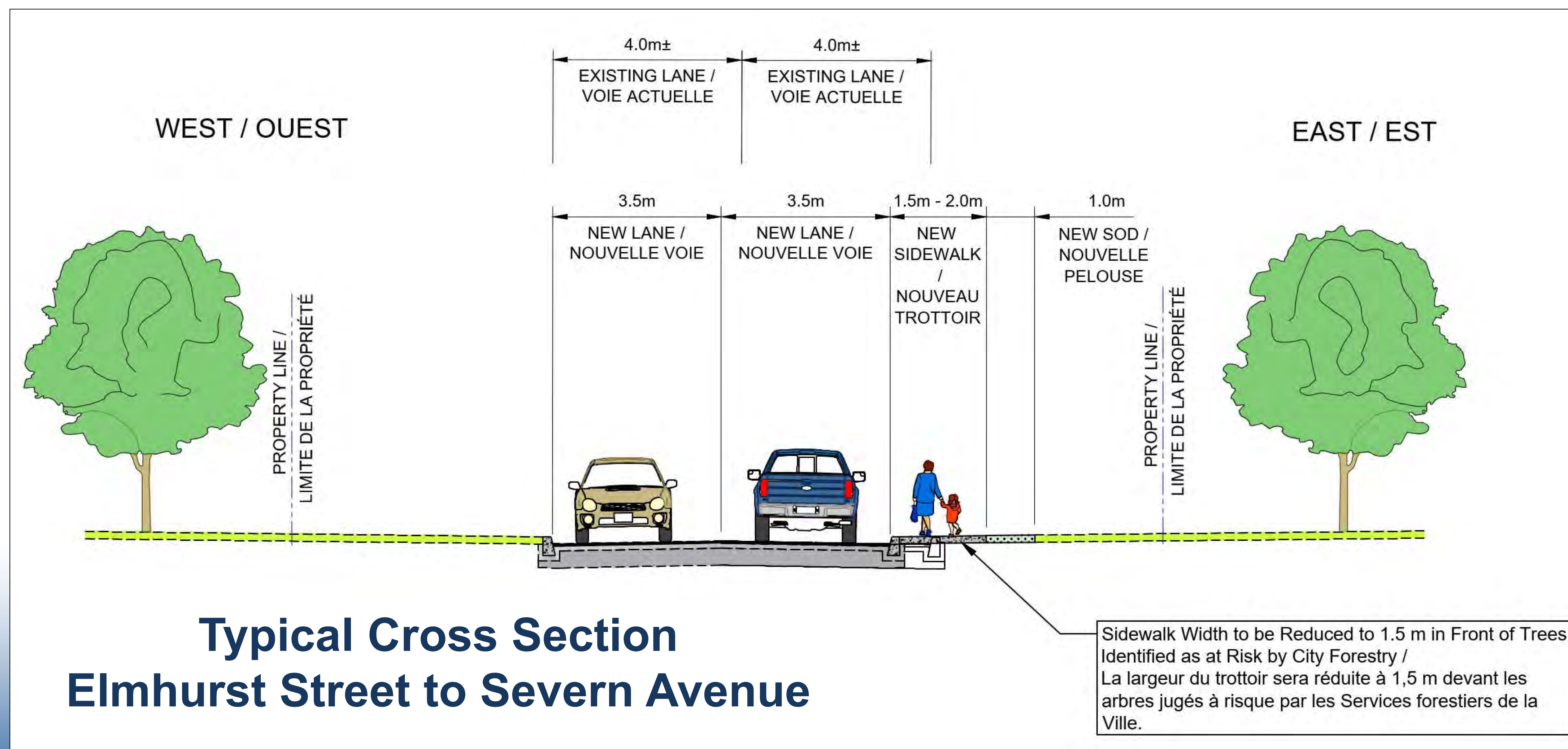
Option 2:

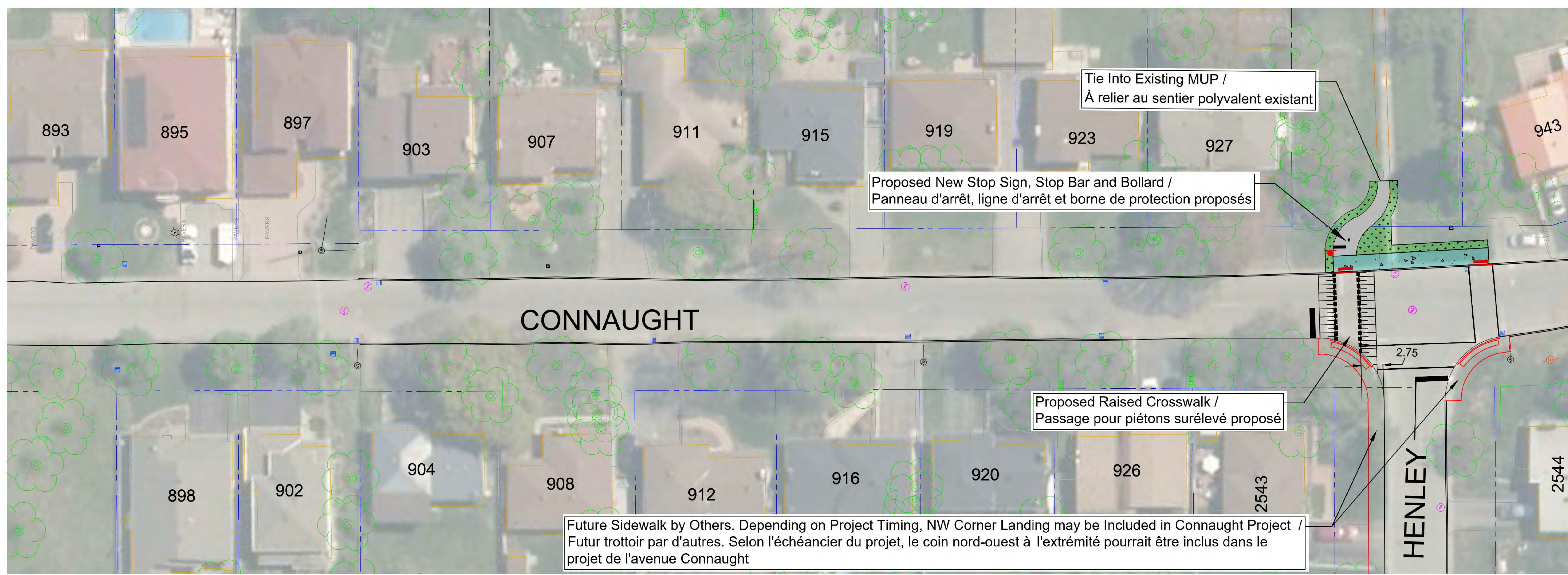
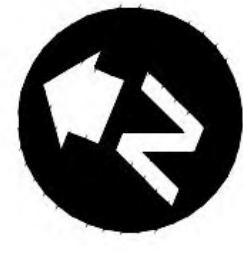
North of Sackville Street to south of Elmhurst Street





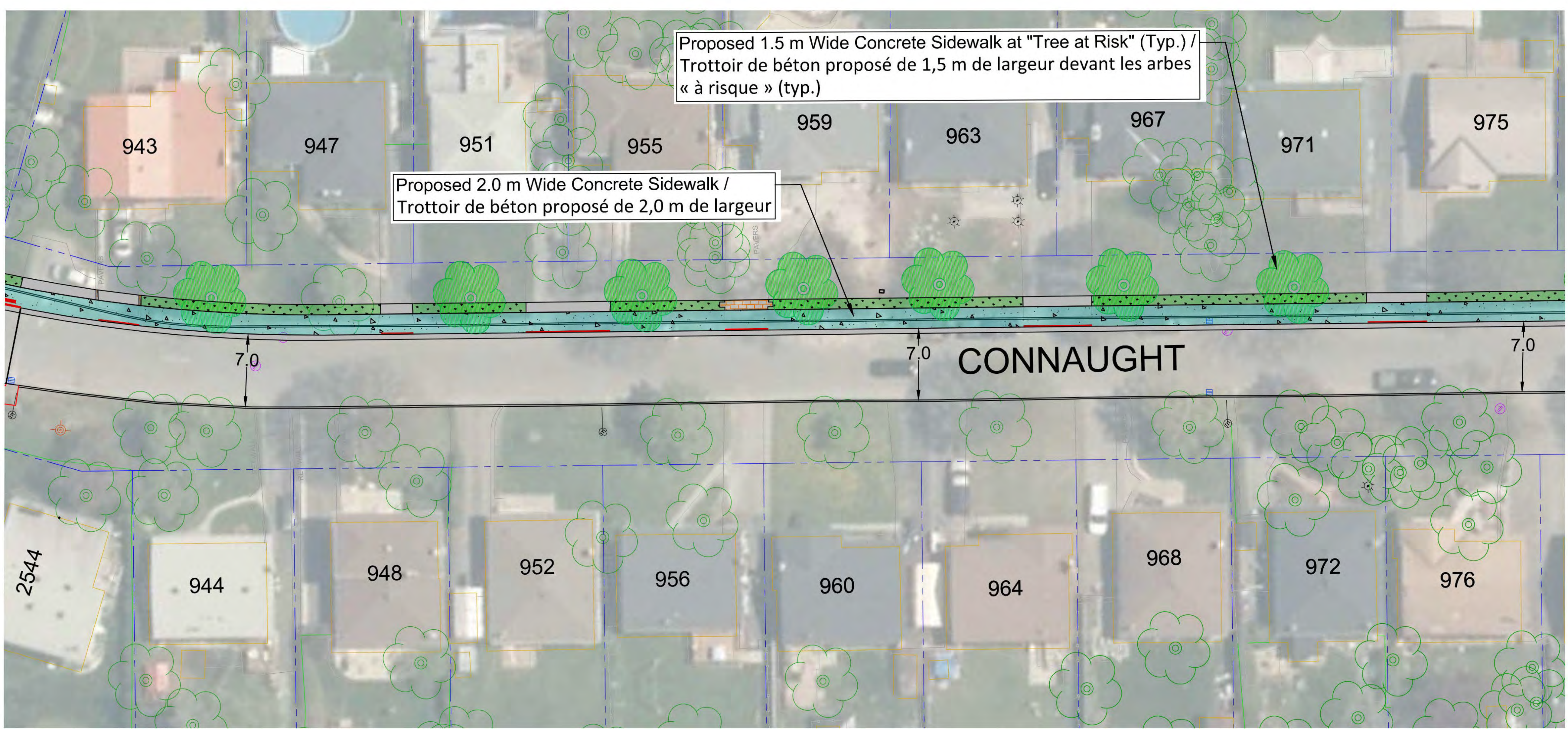
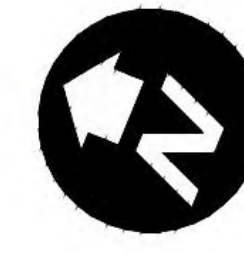
Option 1: South of Elmhurst Street to south of Henley Street



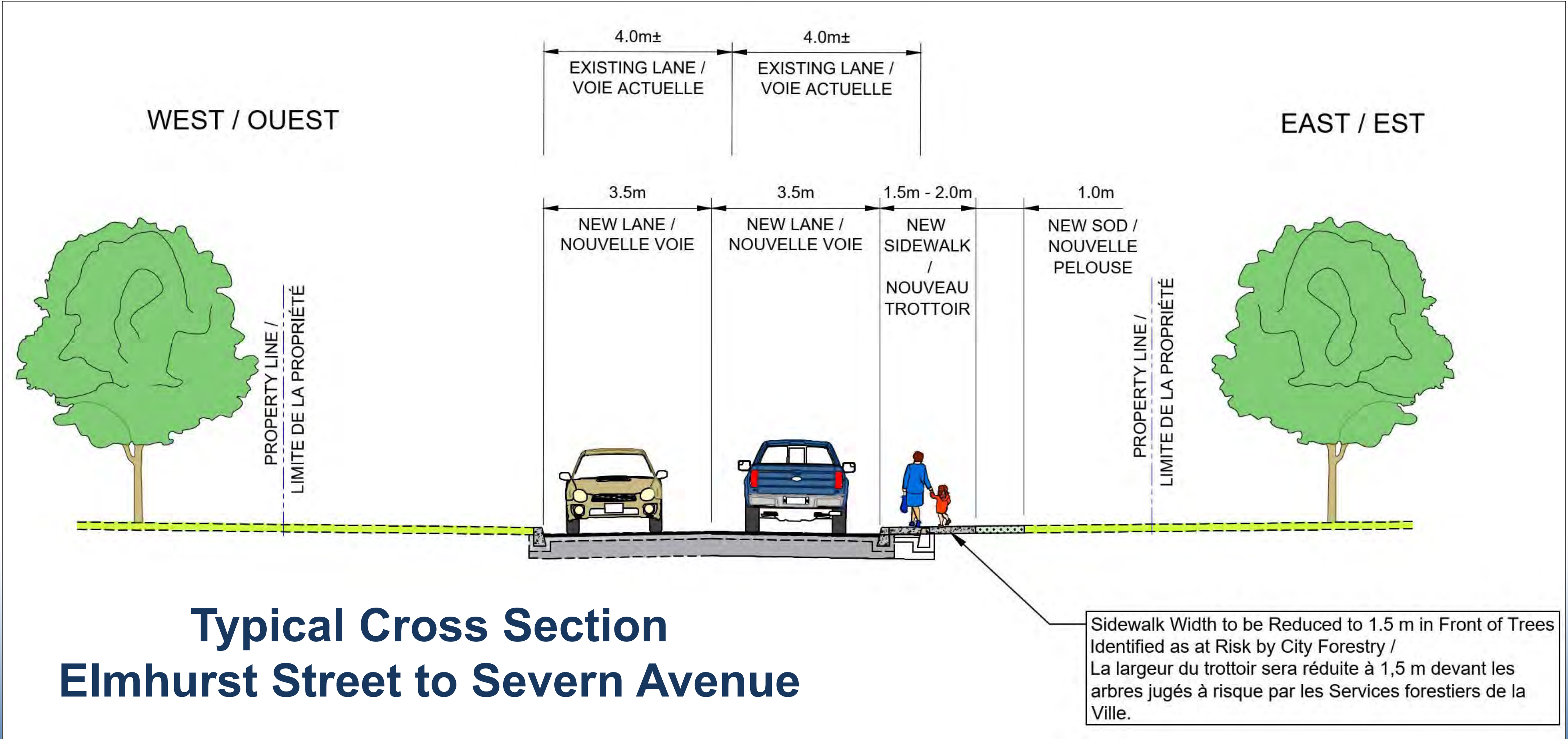


Option 2:

South of Elmhurst Street to south of Henley Street



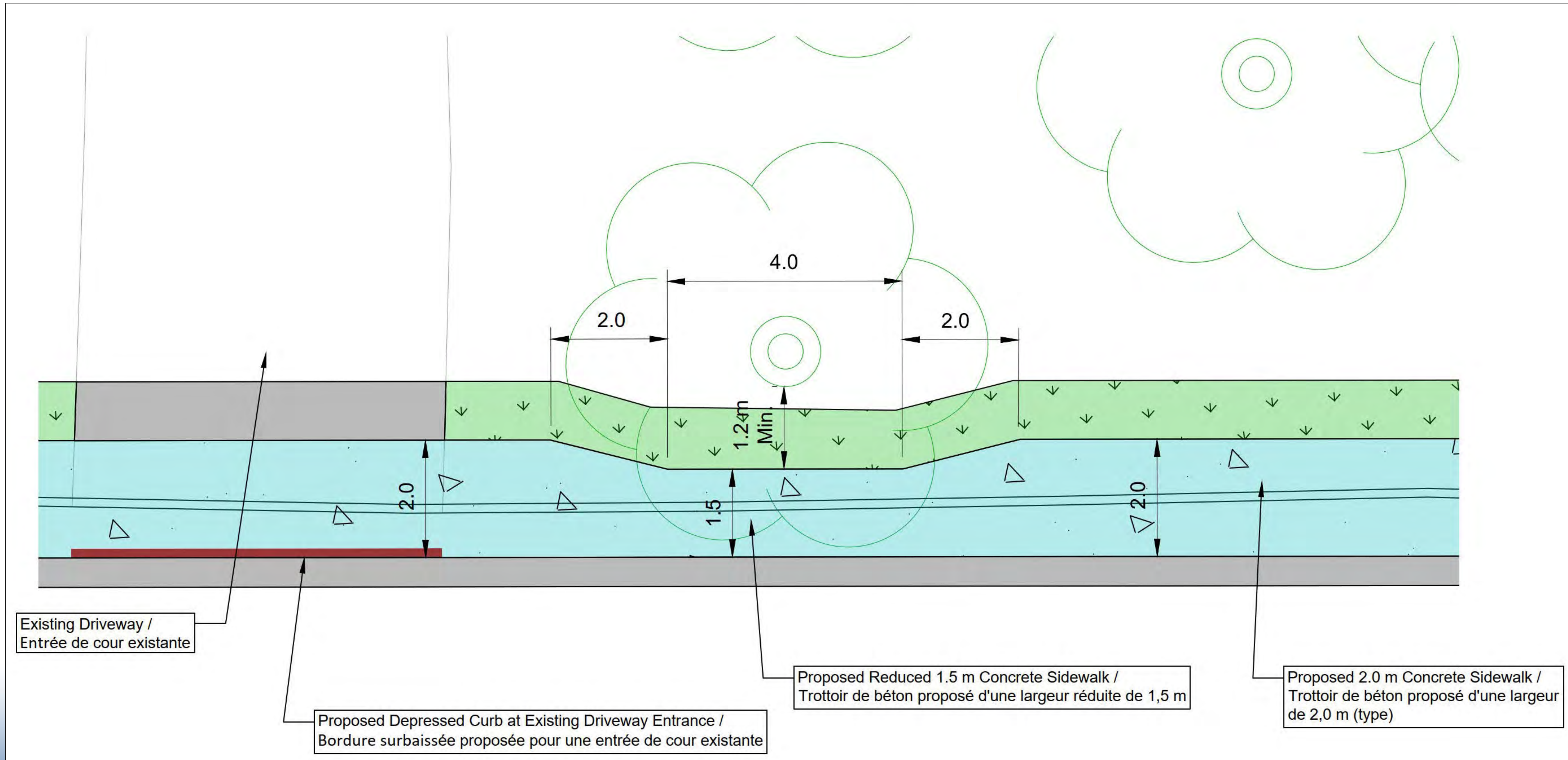
Option 1: South of Henley Street to north of Severn Avenue

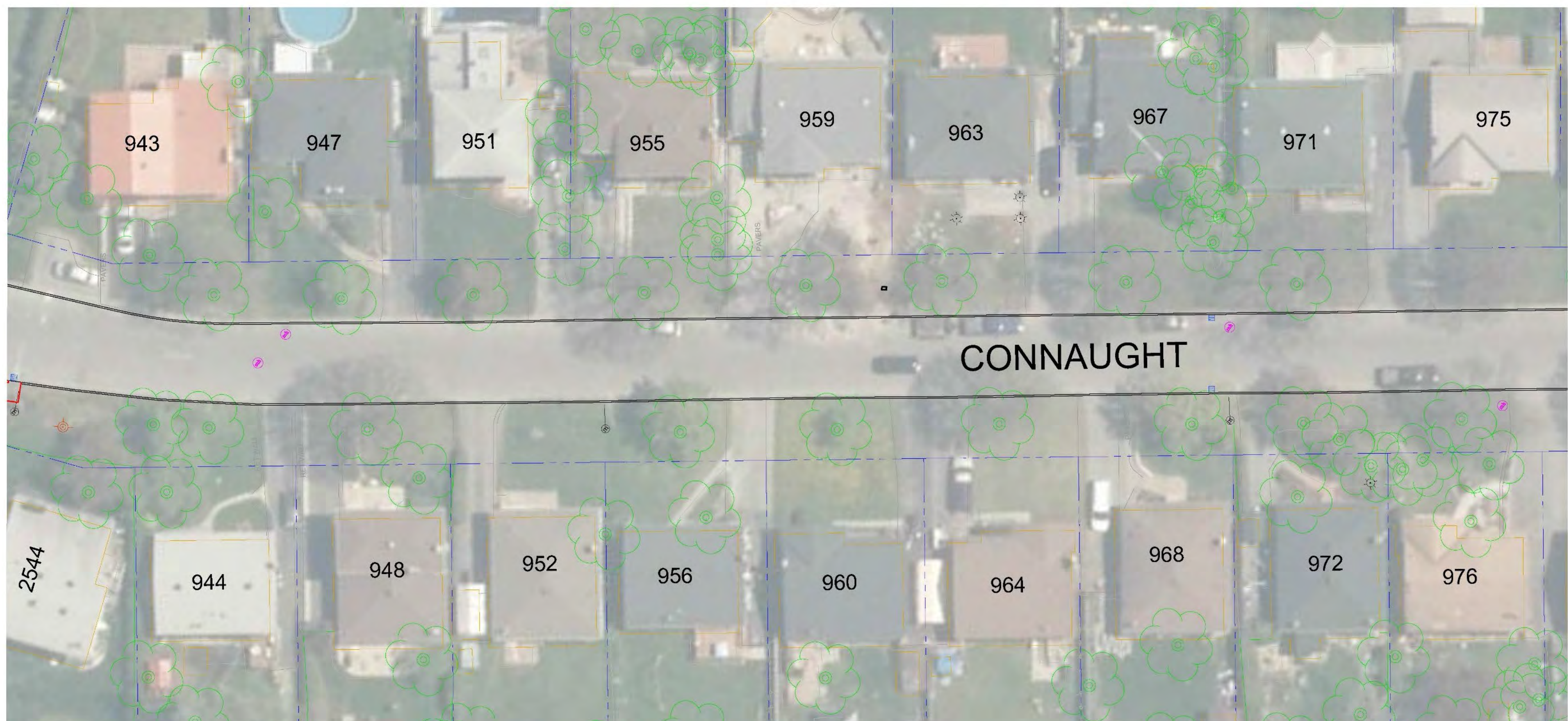


**Typical Cross Section
Elmhurst Street to Severn Avenue**

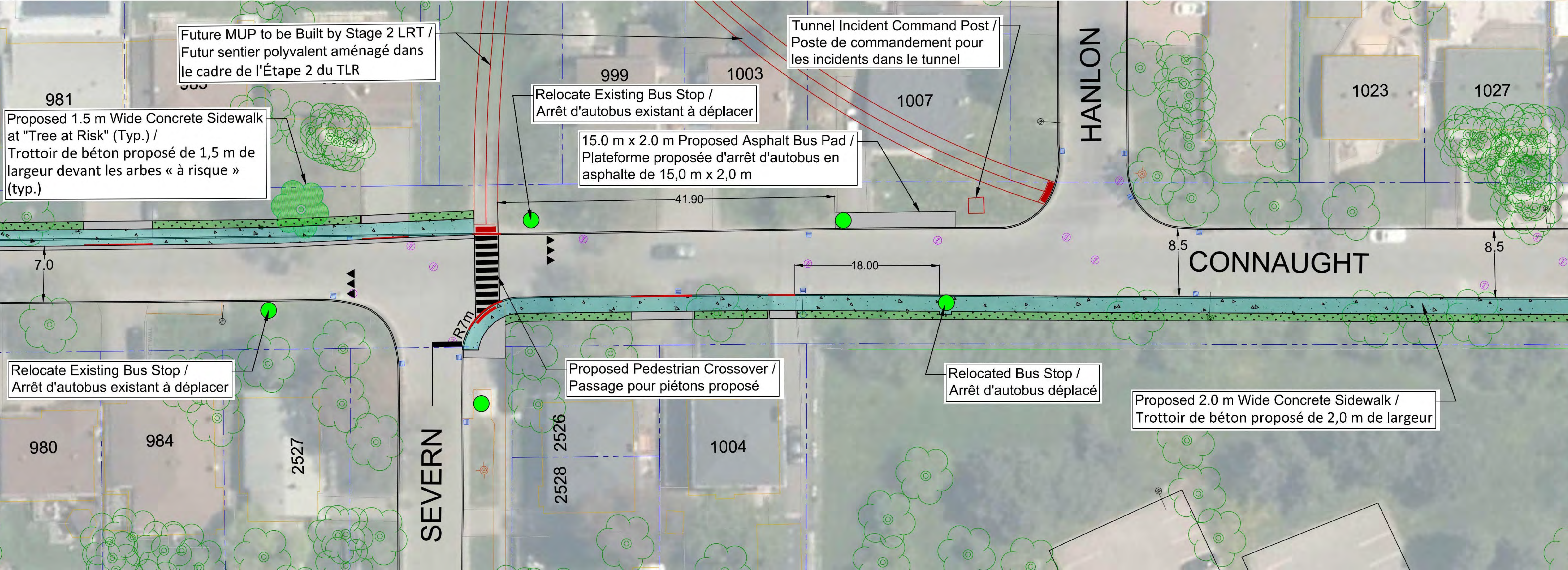
Sidewalk Width to be Reduced to 1.5 m in Front of Trees Identified as at Risk by City Forestry / La largeur du trottoir sera réduite à 1,5 m devant les arbres jugés à risque par les Services forestiers de la Ville.

Option 1: Sidewalk Detail at “Tree At-Risk” as identified by City Forestry Between Elmhurst Street and Severn Avenue

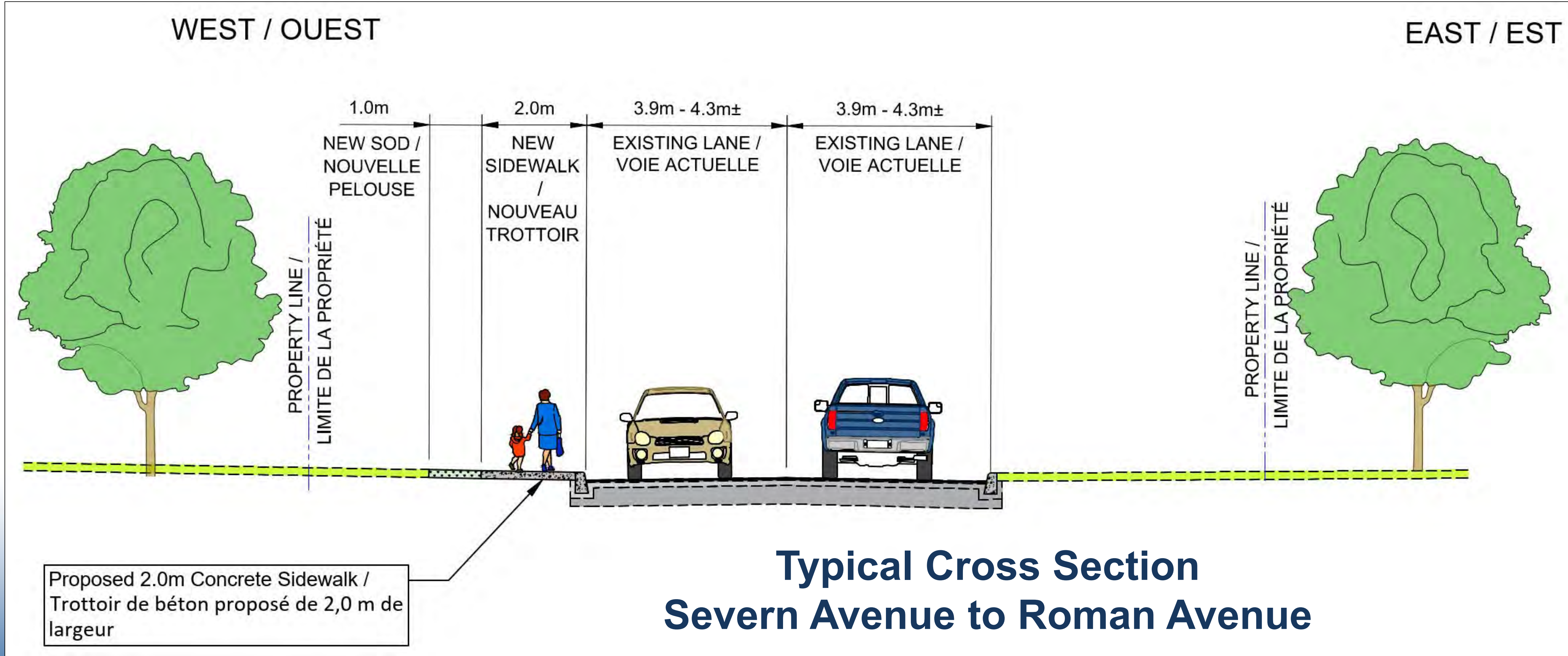


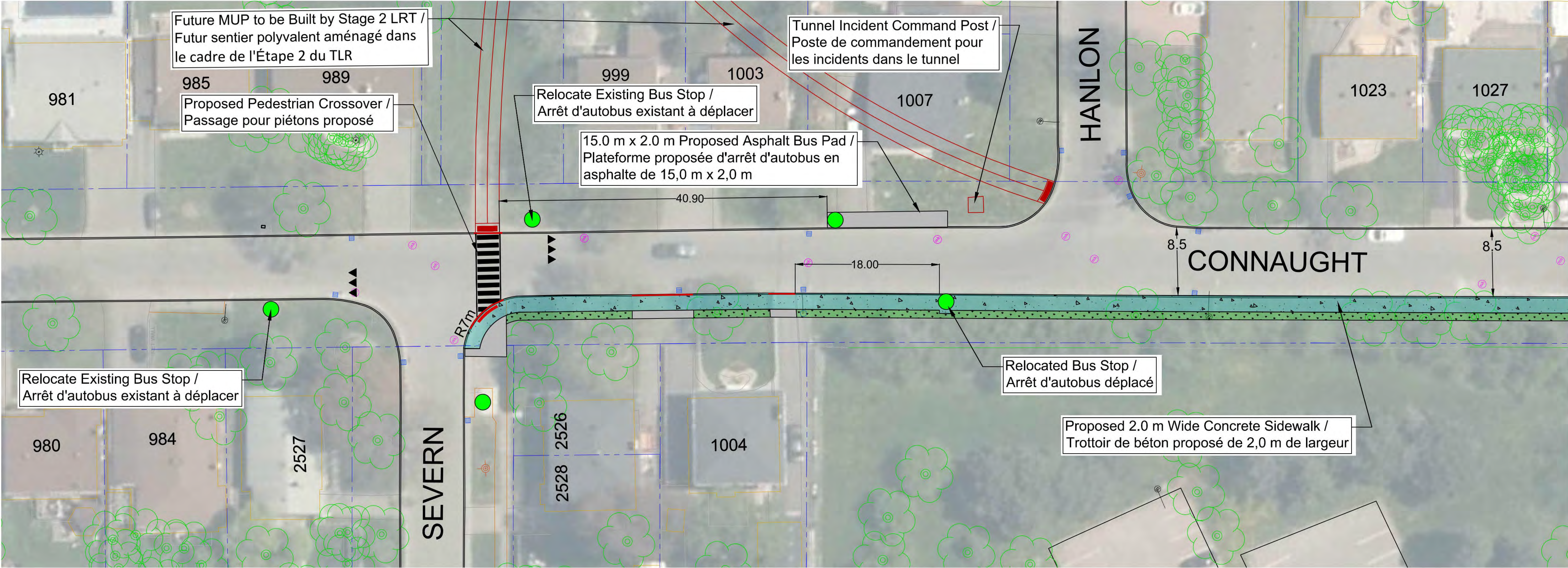
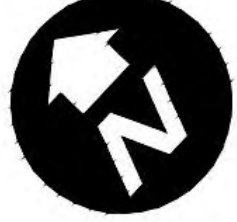


Option 2:
South of Henley Street to north of Severn Avenue
(No modifications proposed)

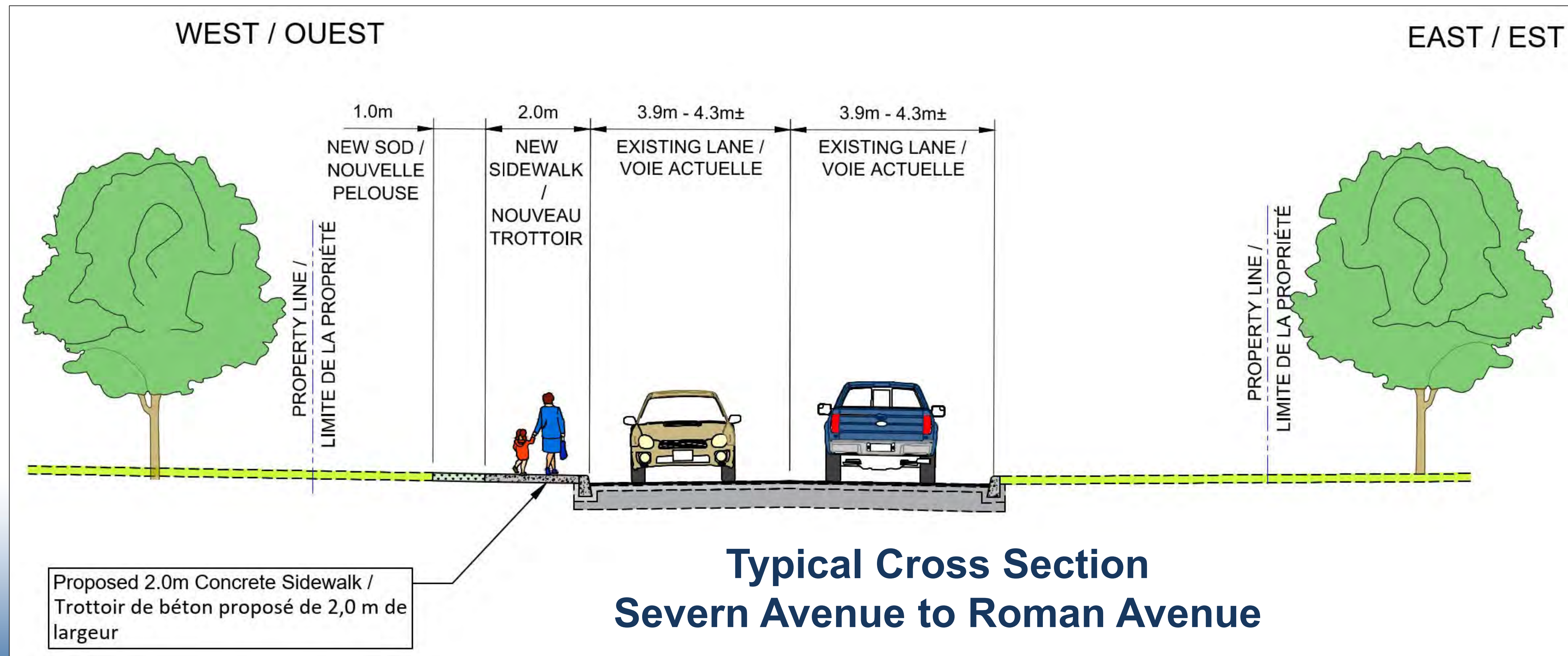


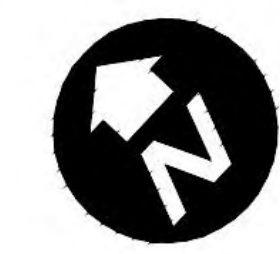
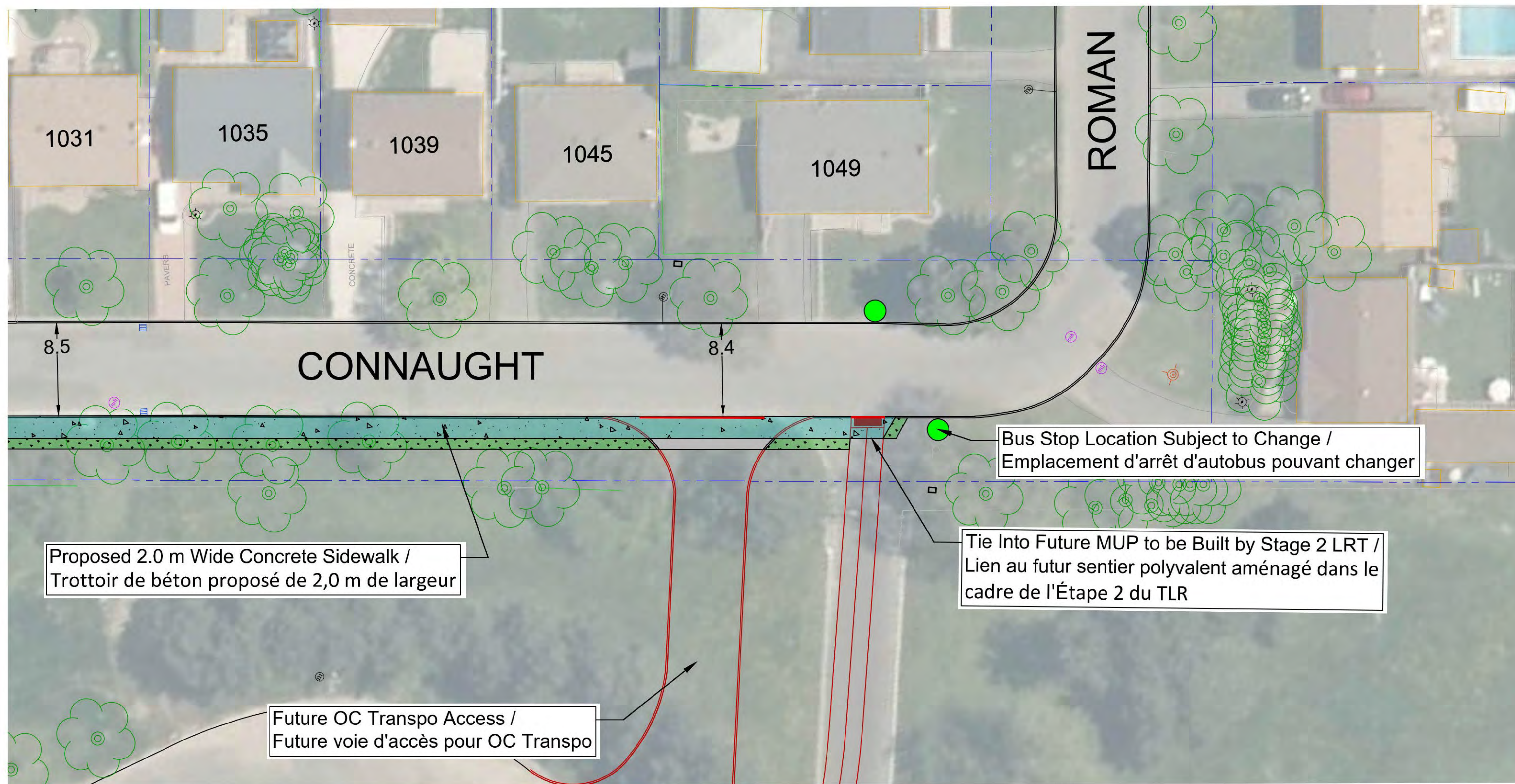
Option 1: North of Severn Avenue to south of Hanlon Avenue





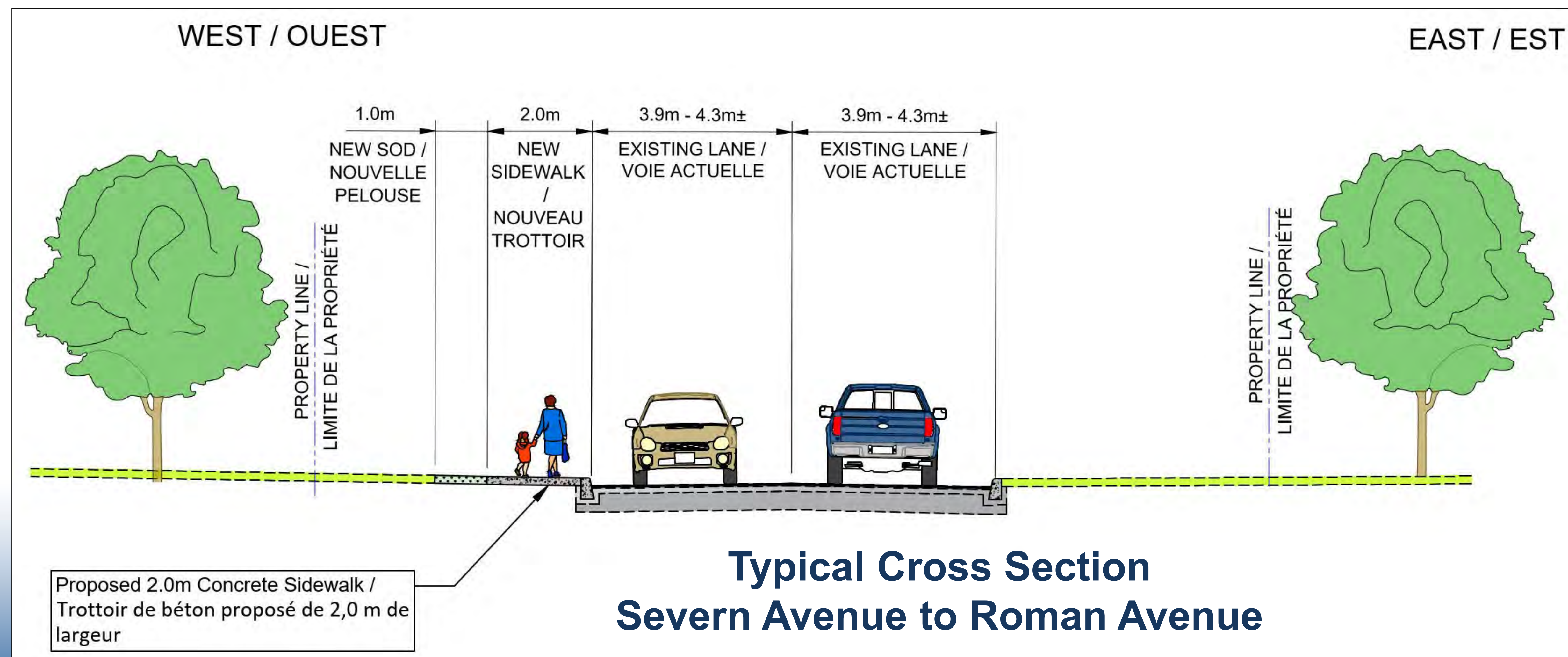
Option 2: North of Severn Avenue to south of Hanlon Avenue





Options 1 and 2:

South of Hanlon Avenue to Roman Avenue



Other New Sidewalks in the Vicinity of Connaught Avenue: Henley Street Sidewalk / Queensway Terrace Storm Sewer Rehabilitation

- The City of Ottawa will be commencing construction on the storm sewer rehabilitation project within the Queensway Terrace North Community to improve the overall performance of the storm sewer system.
- Associated with this storm sewer project is a new sidewalk that will be built along the north side of Henley Street, from Alpine Avenue to Connaught Avenue.
- Construction timing of the sidewalk will occur during the later stages of the storm sewer construction work.
- More information on the storm sewer rehabilitation project can be found on the City of Ottawa website: <https://ottawa.ca/en/city-hall/public-engagement/projects/queensway-terrace-storm-sewer-rehabilitation>

Next Steps

- Please provide your feedback by completing the online survey linked at ottawa.ca/connaughtavenue or by sending comments by March 18, 2021 to:

Genya Stefanoff, MCIP, RPP

Project Manager, Cycling & Walking Programs

City of Ottawa, Transportation Planning

genya.stefanoff@ottawa.ca

Thank you!