

Ministry of the Environment
and Climate Change

Ministère de l'Environnement
et de l'Action en matière de
changement climatique



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OCT 06 2017

Ms. Angela Taylor
Senior Project Engineer
Transportation Planning Branch
Planning & Growth Management
City of Ottawa
110 Laurier Avenue West
Ottawa ON K1P 1J1

Marc R. Clermont
Director of Public Works
United Counties of Prescott Russell
P.O Box 304
59 Court Street
L'Original ON K0B 1K0

Dear Ms. Taylor and Mr. Clermont:

Between June 16, 2016 and July 7, 2016, I received eight Part II Order requests asking that the United Counties of Prescott-Russell and the City of Ottawa (Proponents) be required to prepare an individual environmental assessment for the proposed Ottawa Road 174/Count Road 17 Improvements (Project).

I am taking this opportunity to inform you that I have decided that an individual environmental assessment is not required. This decision was made after giving careful consideration to the issues raised in the requests, the Project documentation, the provisions of the Municipal Engineers Association's Municipal Class Environmental Assessment, and other relevant matters required to be considered under subsection 16(4) of the Environmental Assessment Act (Act). The reasons for my decision may be found in the attached letters to the requesters.

Despite my not requiring an individual environmental assessment be prepared, during the review it was noted that there are some concerns regarding the need for a noise barrier at a section of the proposed Project. I am imposing the following conditions to ensure that the Proponents allow for further consultation regarding the potential noise mitigation measures for the Project. In addition I am imposing a condition on the City of Ottawa requiring them to complete construction of the Confederation Line Light Rapid Transit (LRT) east and have it fully operational before construction of Ottawa Road 174 between Highway 417 to Canaan Road:

- 1) The Proponent shall consider dedicated cycle lanes as part of the final design for the proposed project.
- 2) Before the City of Ottawa can proceed with construction of Ottawa Road 174, between Highway 417 and Canaan Road, the City of Ottawa must:
 - a. Complete an updated noise study for Ottawa Road 174, between Highway 417 and Canaan Road for any areas identified within the Environmental Study Report where noise levels were modelled in exceedance of 58 decibels (including along Elderberry Terrace), in accordance with the City of Ottawa guidelines for noise.
 - i. Based on the results of the updated noise study, the City of Ottawa will be required to construct as part of the Project a noise barrier along any section of Ottawa Road 174 that the study shows exceeds the noise standard for the City of Ottawa in effect at the time construction is proposed to proceed.
 - b. Hold at least one public meeting, during the detailed design phase of the Project. The purpose of the meeting is to discuss the final design of the Project, including noise barriers and designated cycle lanes.
- 3) Following the completion of construction of the Project proposed for Ottawa Road 174, the City of Ottawa must confirm noise levels for those areas of the Project for which a noise barrier was not constructed and noise levels exceed 58 decibels. If the noise levels in any of these areas exceeds the noise standard for the City of Ottawa in effect at that time, the City of Ottawa will be required to construct a noise barrier for these sections of the Project.
- 4) Before construction of the portion of the Project proposed for Ottawa Road 174, between Highway 417 and Trim Road, begins:
 - a. The LRT East Extension must have been operational for at least 12 months; and
 - b. The City of Ottawa must complete a review of the need for the widening from two lanes to three lanes in each direction along Ottawa Road 174, between Highway 417 and Trim Road.
- 5) The results of the review required in Condition 3 above shall be documented in a report, and shall, at least 60 days prior to the start of construction:
 - a. Be posted on a publicly accessible website; and
 - b. Submitted to the Director of the Environmental Approvals Branch.

- 6) For the purposes of these conditions, construction will be deemed to have started the earlier of:
 - a. Tendering of the Project; or
 - b. Physical construction activities starting, including site preparation.
- 7) For the purposes of these conditions, "noise standard" means the noise level in dba at which the City of Ottawa must consider noise mitigation measures.
- 8) If commencement of construction of the proposed Project between Highway 417 to Canaan Road exceeds 10 years from the Minister's decision of the Part II Order requests, the City of Ottawa shall review the planning and design process and the current environmental setting in accordance with the addendum provisions outlined in the Municipal Class Environmental Assessment to ensure that the Project and the mitigation measures are still valid given the current planning context.
 - a. The City of Ottawa shall submit the review for the Project to the Director of the Environmental Approvals Branch prior to commencement of construction of the Highway 417 to Canaan Road.
- 9) Once conditions 1 to 8 have been satisfied, the Proponents shall notify the Director of the Environmental Approvals Branch.

With this decision having been made, the Proponents can now proceed with the Project, subject to the conditions imposed. The United Counties of Prescott-Russell and the City of Ottawa must ensure the Project is implemented in the manner it was developed and designed, as set out in the Project documentation, and inclusive of all mitigating measures, and environmental and other provisions therein. In the interest in transparency I encourage you to make this letter available to the greater public on the Project website.

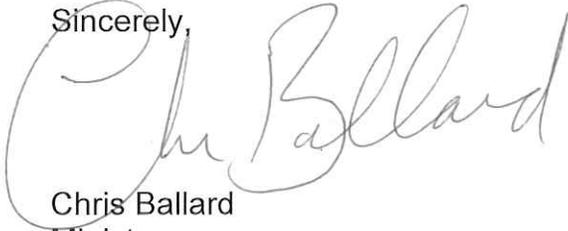
If it is determined that dedicated cycle lanes can be added in areas that were not previously identified in the Environmental Study Report, please feel free to contact ministry staff for additional guidance. Supporting cycling is an important part of Ontario's Climate Change Action Plan. Good cycling infrastructure gets people out of their cars and onto bikes and transit for daily commute, effectively reducing gas pollution while also improving public health.

Lastly, I would like to ensure that the United Counties of Prescott-Russell and the City of Ottawa understand that failure to comply with the Act, the provisions of the Class Environmental Assessment, and failure to implement the Project in the manner described in the planning documents, are contraventions of the Act and may result in prosecution under section 38 of the Act.

Ms. Angela Taylor and Mr. Marc R. Clermont
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I am confident that the proponents recognize the importance and value of the Act and will ensure that its requirements and those of the Class Environmental Assessment are satisfied.

Sincerely,

A handwritten signature in cursive script that reads "Chris Ballard". The signature is written in black ink and is positioned to the right of the word "Sincerely,".

Chris Ballard
Minister

Attachment

c: Requesters
Valeria McGirr, Consultant Project Manager, AECOM
EA File No. 16065
Ottawa Road 174/County Road 17 Improvements